



**City of Healdsburg**  
**Specific Plan for Area A**  
With revision of September 5, 1995

**City of Healdsburg  
Planning Department  
401 Grove Street  
Healdsburg, CA 95448**

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# **1. INTRODUCTION**

# **1 INTRODUCTION**

## **1.1 PREFACE**

The Area A Specific Plan represents an effort spanning six years to create a plan which will provide the framework for future growth and development of 230+ acres in the unincorporated area immediately north of the City of Healdsburg. The plan, which has been developed with a thorough analysis of environmental conditions and extensive input from city decision-makers, landowners, and the community-at-large, provides a comprehensive land use program for the planning area along with goals and policies to guide future public and private actions relating to the area's development. In addition, the plan includes information on necessary infrastructure improvements and costs, and a strategy for insuring the plan's implementation. The plan provides a mechanism to insure that development proposed by planning area landowners will be coordinated and occur in an orderly manner that has been adequately planned.

## **1.2 THE SPECIFIC PLAN**

The City of Healdsburg has determined that specific plans are the appropriate planning mechanism to guide long-range development of Area A and other unincorporated areas within the City's Urban Service Area. A specific plan is a planning and regulatory tool made available to local governments by the State of California. By law, specific plans are intended to implement a city or county's general plan through the development of policies, programs and regulations which provide an intermediate level of detail between the general plan and individual development projects. As vehicles for the implementation of the goals and policies of a community's General Plan, State law requires that specific plans can only be adopted or amended if they are consistent with the jurisdiction's adopted General Plan.

The authority to prepare and adopt specific plans and the requirements for its contents are set forth in the California Government Code (Planning and Zoning Laws), Title VII, Chapter 3, Article 8, Sections 65450 through 65457. The law requires that a Specific Plan include text and diagrams specifying:

- the distribution, location, and intensity of land uses, including open space, within the plan area;
- the distribution, location, and capacity of infrastructure, including transportation, water, storm drainage, solid waste, and energy systems;
- design standards and criteria for development and use of natural resources; and
- an implementation program, including capital improvements plans, regulation and financing strategies.

Adoption of this Specific Plan will provide the framework for the future land uses and guide the form of new development in Area A through to ultimate buildout of the area. The adopted Plan will provide a public planning document that will act as an extension of the General Plan, and be used as both a policy and a regulatory document. When private development proposals for the planning area are brought before the City, the planning staff will use the Specific Plan as a guide for project review. Projects will be evaluated on their consistency with the intent of Plan's policies and conformance with development standards and design guidelines. For projects within Area A, policies and standards in the Specific Plan will take precedence over more general policies and standards applied throughout the rest of the City. In situations where policies or standards relating to a particular subject have not been provided in the Specific Plan, the existing policies and standards of the City's General Plan and Zoning Ordinance will continue to apply.

The adoption of the Area A Specific Plan constitutes a project under the California Environmental Quality Act (CEQA). An accompanying Environmental Impact Report (EIR) has, therefore, been prepared by Jones & Stokes Associates, Sacramento, to disclose the potential direct and indirect environmental effects associated with buildout of the area. Although the environmental documentation has been included in a separate document, it is important to note that the environmental review process has been an integral component of the planning process from the very beginning to ensure sensitivity to critical environmental concerns. To keep the Specific Plan as concise as possible, much of the environmental data has not been included in the plan document. For additional information relating to the environmental foundation of the plan one should refer to the "Area A Specific Plan EIR".

All lot layouts as shown in the figures included in this Specific Plan are tentative and schematic. More precise plans for lot layouts will be submitted as part of the tentative subdivision map applications. Further environmental review under the California Environmental Quality Act (CEQA), including formulation of appropriate mitigation measures for any identified impacts, will be required at such time.

### **1.3 ORGANIZATION OF THE PLAN**

The Specific Plan is organized to provide a step-by-step understanding of the Plan's components and the rationale behind its policy recommendations, design concepts, and implementation measures. The first five chapters are primarily descriptive. The policies, standards, guidelines, and implementation measures which will regulate the plan are presented in subsequent chapters. These policies, standards, guidelines, and implementation measures are organized in a series of "Elements" that correspond to categories established by City and State General Plan guidelines.

Chapters in the Specific Plan include:

- 1. Introduction**—establishes the broad purpose of the Specific Plan, describes the legislative authority under which specific plans exist, outlines the organization of the Plan.
- 2. Planning Context**—summarizes the general conditions and sequence of events leading up to the Plan's preparation.
- 3. Planning Area Description**—describes the location and general character of the planning area, and identifies key environmental factors that influenced the Plan's form and policies.
- 4. Specific Plan Goals**—states the fundamental intentions identified for Area A.
- 5. Plan Summary**—sets forth the rationale and conceptual framework underlying the Plan's physical form, land use program, development standards and design guidelines, infrastructure improvements, and implementation strategies.
- 6. Land Use Element**—translates planning goals and framework concepts into land use patterns and associated development objectives and policies.
- 7. Housing Element**—establishes objectives and policies relating to residential mix, housing affordability and development character.
- 8. Open Space Element**—describes the open space network within Area A and associated policies, including those relating to resource protection and public use.
- 9. Circulation Element**—describes the circulation network and identifies the components and design standards required to accommodate efficient access and movement of vehicles and pedestrians in and around Area A.
- 10. Public Facilities Element**—locates and characterizes public facilities anticipated in Area A, including schools, sewer, water, drainageways and stormwater detention areas, and sets forth related policies.
- 11. Urban Design Element**—sets forth design concepts, policies and objectives, and translates them into standards and guidelines for streets, yards, open space, grading, siting, landscaping, buildings and other physical features.

- 12. Implementation Element**—describes policies, regulations and ordinances that must be adopted or amended to implement the plan, and identifies development approval procedures, capital improvements, financing programs, and development phasing recommendations.
- 13. Financial Element**—describes cumulative fiscal impact on City revenues, financial and phasing plans for on site infrastructure and oversizing and off-site capacity projects, assessment district feasibility evaluation and associated mitigation measures required. This element is contained in Volume 2 of the Specific Plan.

## **2. PLANNING CONTEXT**

# **2 PLANNING CONTEXT**

## **2.1 BACKGROUND AND NEED FOR PLAN**

The City of Healdsburg lies at the northern end of Sonoma County's rapidly growing Highway 101 corridor, and has increasingly been experiencing pressures for growth. Since 1980, the City has been growing at an average annual rate of 2.5 percent. Growth is expected to continue through the year 2010. Association of Bay Area Government's (ABAG) projection (revised for 1992) estimate a population for Healdsburg in 2010 of 15,160. The City's own projections estimate 13,776 population for the same year. The prospect of this growth has raised concern in the City that the character and quality of life in Healdsburg could be compromised if growth is not adequately planned.

A primary land use goal of the Healdsburg General Plan is "to provide for orderly development within well-defined urban boundaries" (Goal A, Land Use Element, p. 19).

In support of this goal the City has established as policy that urban development can only occur within the City limits, and that no or development of annexed areas can occur until a specific plan has been prepared and adopted for the area. These specific plans will be required to comply with the requirements of State law and address in detail the proposed land use pattern, circulation system and other improvements, phasing of development, and financing of infrastructure improvements. The General Plan specifically identifies the northern part of the Urban Service Area, which includes Area A, as an area requiring the preparation of a specific plan(s). It also specifies that no more than three specific plans should be developed to cover the northern planning area, and that, if more than one plan is developed, each plan will consider the cumulative effects of development of the entire area.

Due to its location adjacent to Healdsburg's present urbanized area, Area A appears poised for change. Future annexation and development is anticipated for Area A as major landowners have expressed an interest in pursuing development. Development of the northern planning area will redefine Healdsburg's northern edge, creating a major addition to Healdsburg's rich urban fabric and establishing the City's newest neighborhood.

## **2.2 PLANNING PROCESS**

In 1988, the City of Healdsburg with Z.A.M. Inc. (landowners with holdings in Area A) acting as lead agent for the City commissioned Wallace Roberts & Todd, San Francisco to develop the Area A Specific Plan pursuant to existing General Plan policies. To assist in the process, Jones & Stokes of Sacramento was retained by the City to perform the necessary environmental baseline analysis and environmental assessments of Plan alternatives and the final Plan.

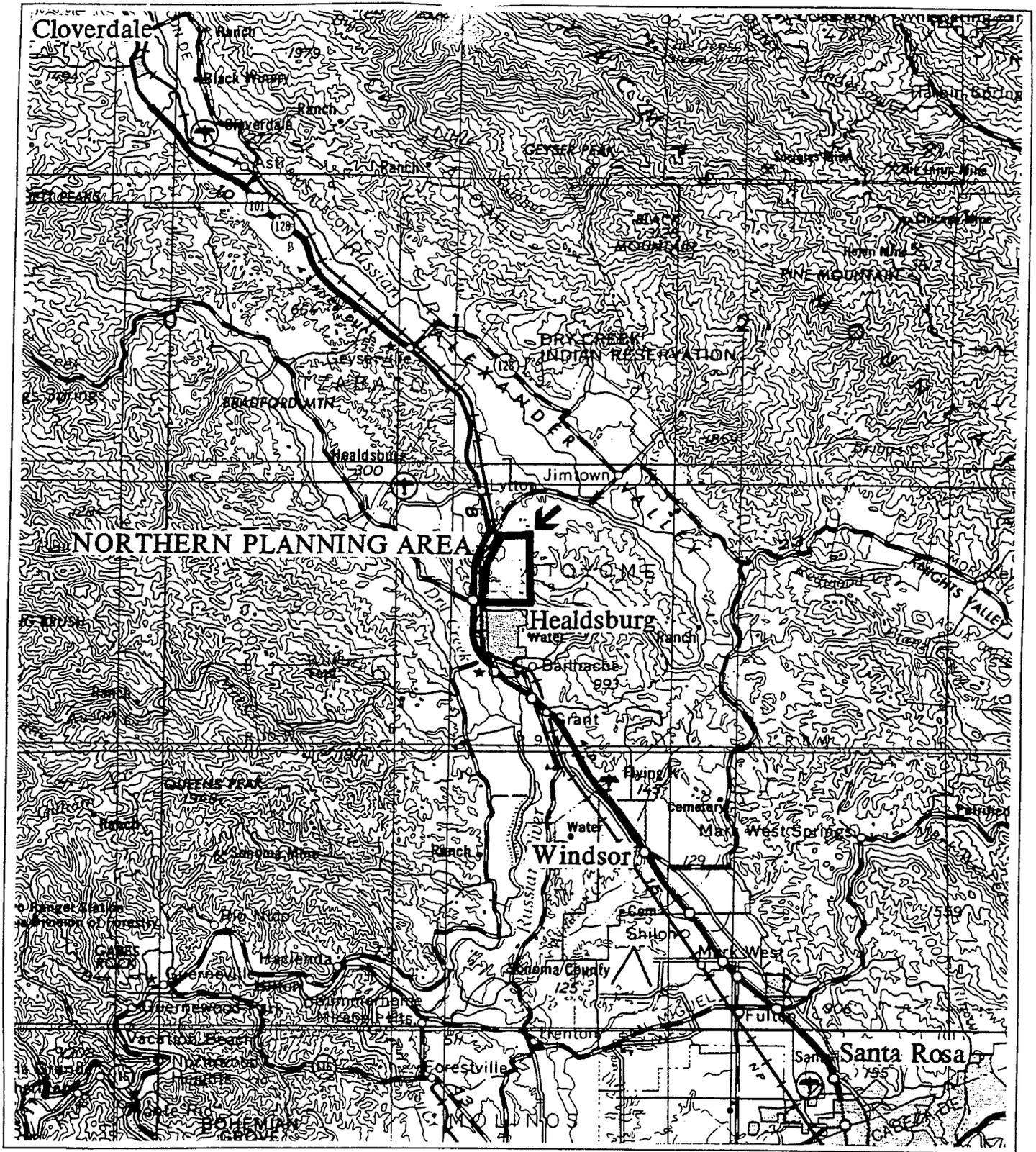
Development of the Area A Specific Plan has involved a process of data collection, environmental analysis, alternatives development and evaluation, and plan development. After initial data collection, Jones & Stokes prepared a background report summarizing environmental constraints and opportunities affecting development within the planning area. The report also provided recommendations for minimizing environmental impacts through project planning and design.

Based on the information provided by this analysis, three initial alternative plans showing different densities and distribution of uses were developed by WRT. An abbreviated environmental assessment was conducted on each of these alternatives to determine the relative impact potential of the three land use scenarios. This assessment process was used to determine appropriate locations and intensities for development, to identify appropriate open space areas, to formulate appropriate development standards and design guidelines, and to conceive a comprehensive design concept that is integrally linked with environmental factors. Following this assessment, Jones and Stokes prepared a fourth alternative land use plan to depict a development pattern which would have afforded more protection to the natural environment.

City Council, City staff, planning area landowners and citizens participated in the review of the four alternatives in a series of meetings. Input from these meetings served as the basis for preparing a fifth alternative land use plan which became the preferred land use scenario for Area A. The preferred alternative was then further revised with input from affected property owners, City staff and Jones and Stokes, and the policies, guidelines, and regulations for implementing the plan were developed.

A Draft Specific Plan and the accompanying EIR were subject to public review and a number of public hearings before the Planning Commission and the City Council. As a result of this review, the City Council requested that the Draft Specific Plan be modified based on several policy decisions. The accompanying EIR was also required to be revised to reflect these changes in the Specific Plan. Both the revised Draft Specific Plan and the accompanying revised EIR were subject to a final public review and public hearing before the City Council prior to the City Council approving the plan and certifying the EIR as complete and adequate on November 21, 1994.

### **3. PLANNING AREA DESCRIPTION**



Scale 1" = 3 miles

**REGIONAL SETTING**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
**AREA A**

Wallace Roberts & Todd  
 Jones & Stokes Associates

Source: U.S.G.S.

Figure 3A

# **3 AREA A DESCRIPTION**

## **3.1 LOCATION**

The City of Healdsburg is located in northern Sonoma County, at the intersection of three rich agricultural valleys: Sonoma Valley, Dry Creek Valley, and Alexander Valley. The City is situated along the Highway 101 corridor, approximately 18 miles north of Santa Rosa and 8 miles north of Windsor. Located just north and west of Healdsburg are the predominantly agricultural, grape-producing areas of Alexander Valley and Dry Creek Valley. The small community of Geyserville lies about 8 miles to the north, and Cloverdale is approximately 18 miles to the north.

*(Figure 3A)*

The 230+ acre planning area, Area A, is located immediately north and east of the City's current boundary, with Healdsburg Avenue forming the western boundary of the planning area and residential development bordering the area on the south. Area A is one of three areas (Areas B and C being the other two) that comprise the City's Northern Planning Area. The Northern Planning Area consists of approximately 850 acres of unincorporated land located between the incorporated city limits and the City's current Urban Service Line. Areas B and C are roughly the same size as Area A. Area C is adjacent to the north of Area A, and Area B is adjacent to the east side of Area A.

Due to its location adjacent to Healdsburg Avenue and the urbanized area, Area A is considered by the City to be a primary area for expansion. The entire Northern Planning Area represents a potential transition area between urbanized Healdsburg to the south and west, and open space to the north and east, establishing a new northern gateway entrance into the City. Commercial and industrial enterprises, including the Simi Winery and RJW Lumber currently operate west of Healdsburg Avenue, Area A's western boundary. To the north and east, agricultural lands and open space predominate. The Russian River lies immediately east of Area B.

*(Figure 3B)*

The Northern Planning Area, including Area A, is located about 2 miles north of Healdsburg Plaza, the commercial and psychological center of Healdsburg. The traditional town center layout contributes to the historic, picturesque character for which Healdsburg is widely known. The Plaza, which is located within a traditional grid pattern of streets, is surrounded by Healdsburg's most attractive and historic residential neighborhoods. As one moves away from the town center, recently developed "suburban-style" residences predominate in neighborhoods along the City's edge, including the neighborhood just south of Area A.

## **3.2 GENERAL CHARACTER**

From Healdsburg Avenue, (the Northern Planning Area's western boundary) a rich, varied landscape can be seen: rolling hills with native oaks, chaparral ridgetops, a grassy valley traversed by creeks, and mixed forests. A ridge running southeast to northwest through Area A divides the flat valley lands from the rolling hills along Healdsburg Avenue.

*(Figure 3C)*

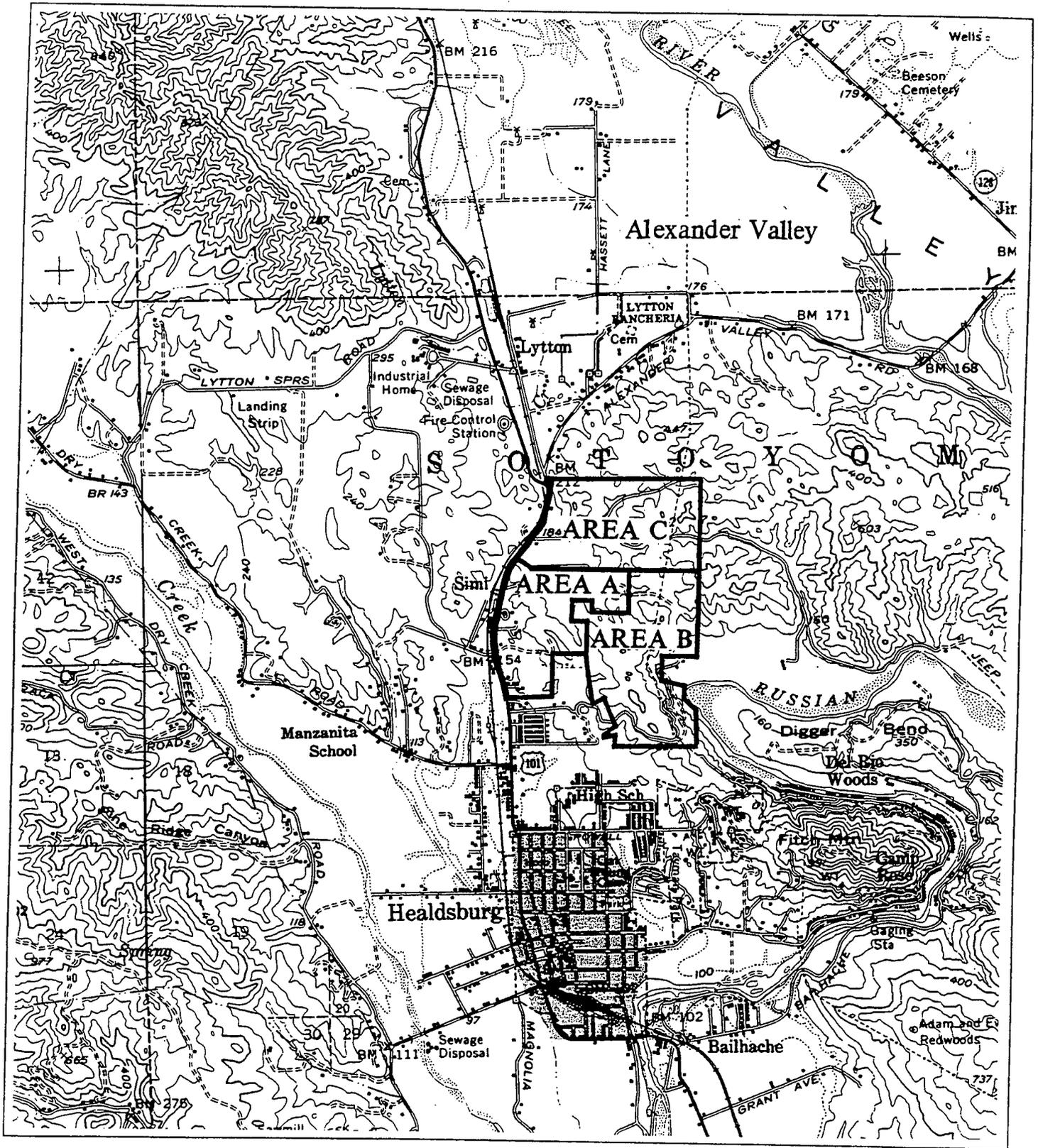
The Northern Planning Area is generally separated from existing urbanized areas by a series of spur ridges that cross the southern portion of Area A. These southwest-northeast trending spur ridges stem from the dominant northwest-southeast main ridge. The rounded stem ridges, with their moderate to steep side slopes and intervening swales and small drainages, comprise the southern two-thirds of Area A. The northern third of Area A consists of a portion of a broad alluvial valley which slopes gently westward. This small valley straddles the Area A-Area C boundary and comprises the largest contiguous flat area within Area A. Elevations within Area A range from approximately 360 feet on the main ridge crest near the eastern boundary to approximately 180 feet in the valley



**NATURAL FRAMEWORK  
NORTHERN HEALDSBURG SPECIFIC PLAN  
AREA A**

Wallace Roberts & Todd  
Jones & Stokes Associates

Figure 3C



Scale 1" = 4000'

**LOCAL SETTING**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
**AREA A**

Wallace Roberts & Todd  
 Jones & Stokes Associates

Source: U.S.G.S.

Figure 3B

at the northwestern corner of the site.

This terrain supports rich and varied habitats. Riparian woodlands occur along creeks in the valley and between the ridges of southern Area A. The southern ridglands also contain the greatest concentration of oak woodland and needlegrass grasslands. Serpentine chaparral exists on ridges in Area A's westernmost areas.

In addition to the site's natural features, the character of the area is also influenced by human habitation and cultivation. A vineyard and several single family homes are located in the southwest portion of Area A near Healdsburg Avenue. Farther north is a bed and breakfast inn and a mobile home, all of which take access from Healdsburg Avenue via dirt roads. Agricultural uses occupy the north and northeastern portions of the site. Overall, the plan area consists of 27 parcels which are owned by 24 landowners. Parcel sizes range from less than one acre to greater than 50 acres, with parcels generally being smaller in the southern portion of Area A and gradually increasing in size from south to north.

### **3.3 ENVIRONMENTAL SETTING**

Area A has a number of important environmental factors which have influenced the planning process. These environmental factors represent both opportunities and constraints to the ultimate development of Area A. The most significant of these factors are summarized below.

#### **3.3.1 GEOLOGY AND HYDROLOGY**

The Northern Planning Area contains a number of geological conditions that potentially could jeopardize public health and safety. Area A contains landslides, steep slopes, seismic faults and other areas of potential instability. These potential hazard areas will constrain development in varying degrees.

(Figure 3D)

The topography of the Northern Planning Area also influences the anticipated form and intensity of development. In general, the flatter areas are typically more suitable for development. Steep slopes are less suitable because of the need for grading and its associated costs, residual geological risks, and aesthetic implications. The steeper areas are also the areas most likely to contain landslides and unstable conditions are most likely to occur.

Area A contains a pond, wetlands, a perennial creek and several intermittent creeks which will affect development decisions. High flows and localized flooding can be expected along the perennial stream in the valley in the northern third of Area A and in the flat-lying drainage area at the southwestern corner during intense, long duration storms. In addition, increased runoff into Foss Creek will exacerbate existing downstream flooding problems.

#### **3.3.2 BIOLOGY**

The Northern Planning Area contains rich and diverse biological resources that are valuable, especially in light of their proximity to urbanized Healdsburg. Poorly located development and inappropriate activities would reduce or destroy these valued habitats. Habitat areas with high biological value deserving protection include:

- serpentine chaparral;
- riparian woodland;
- mixed evergreen forest;
- oak woodland where adjacent to evergreen forest and needlegrass grassland;
- needlegrass grassland where adjacent to oak woodland.

(See "Area A Specific Plan EIR" for further descriptions of these habitat areas.)

(Figure 3E)

### **3.3.3 SCENIC RESOURCES**

Scenic resources play an important role in defining Healdsburg's character, and will significantly influence the form and character of new development. Area A contains major scenic ridgelines, highly visible hillsides, major stands of mature, native vegetation, and rural (agricultural) open spaces that are valuable scenic resources for the community, and will be of major importance to future residents of the area. The General Plan identifies five major scenic ridgelines in Area A that should be preserved and enhanced in order to maintain the area's natural, scenic character. The importance of protecting Area A's scenic resources is magnified by the site's visibility from Highway 101 and Healdsburg Avenue, both of which are designated as scenic roads in the Healdsburg General Plan and both of which are considered major gateways to Healdsburg and the county. Disruption of the site's visual character, therefore, has the potential to be seen by significant numbers of people, thus creating not only a community-wide, but also a region-wide effect.

*(Figure 3F)*

### **3.3.4 NOISE**

Existing noise levels on the site do not appear to pose significant constraints on development, although they may affect the development in portions of Area A. The major noise source is traffic on Healdsburg Avenue. Current noise levels are estimated to be 60 dB Ldn or greater within 125 feet of Healdsburg Avenue, and about 50 dB Ldn over the rest of the site. Future noise levels are estimated to be 60 dB Ldn within 170 feet of Healdsburg Avenue. The 60 dB Ldn contour represents the upper limit normally considered acceptable for residential development. Thus, traffic noise along Healdsburg Avenue could disturb future residents if development is located too close to the roadway or if residential structures are not properly designed.

### **3.3.5 CULTURAL RESOURCES**

The historic and prehistoric use of Area A represents a cultural resource that must be considered when planning for the development. City policy sets a high priority on preserving Healdsburg's historic resources. Although it is assumed that the site was used by Native Americans, no prehistoric sites have been identified. Eight historical, archaeological sites and historical structures have been identified. All of the sites represent relatively recent history (i.e., within the last 100 years), and the actual significance of the sites is still to be determined. The sites include a farm complex in the southwestern portion of Area A along Healdsburg Avenue, an old house just northwest of the farm complex, the Belle de Jour bed and breakfast inn, a California bungalow-style house at 16038 Healdsburg Avenue, and an abandoned one-and-one-half story house southeast of the inn. The other three sites consist of stone walls and foundations and a refuse scatter.

## **4. PLAN GOALS**

# 4 PLAN GOALS

The list of planning goals outlined below provides the framework used to develop the land use, circulation, capital improvement, financing, and design recommendations set forth in this Plan. The goals are based on:

- Existing City of Healdsburg General Plan policies;
- Input from City staff, area landowners, and community members;
- Environmental findings identified in the *Constraints Analysis* completed early in the planning process;
- Planning and design factors relating to the area's ultimate character, identity and quality of life.

Reflecting community opinion, the goals support the maintenance or enhancement of existing natural features, responsiveness to the historic and cultural development pattern of Healdsburg, and the creation of a high quality of life for present and future residents.

## GOALS

- 1 Accommodate new development within Area A while ensuring a high-quality, desirable and affordable place to live.
- 2 Maintain the pedestrian-oriented, small-town character and identity of historic Healdsburg as the City expands into Area A.
- 3 Use existing natural features, such as landforms, vegetation, and streams as key elements in determining the form and character for future Area A development.
- 4 Design an open space and street system which will provide an established framework for future development.
- 5 Preserve and enhance physical and visual connections to open space and natural lands for existing and future residents.
- 6 Create a comprehensive set of planning and design guidelines for the Northern Planning Area (Areas A, B, and C) which uses historic Healdsburg as its primary reference and ensures that all new development within Area A will be aesthetically compatible.
- 7 Create a plan for Area A which recognizes the site's importance in defining the City's northern edge and providing a gateway entrance into the City of Healdsburg.
- 8 Ensure orderly and fiscally-sound development and provision of City services to the planning area.
- 9 Ensure landowners a reasonable beneficial use of their land which is consistent with City General Plan policy and avoids significant environmental impacts.
- 10 Protect the public's health, safety and welfare.
- 11 Accommodate development in Area A while conserving and enhancing existing natural resources and environmental quality.

## **5. PLAN SUMMARY**

# 5 PLAN SUMMARY

## 5.1 LAND USE CONCEPT

The Area A Specific Plan describes a pattern of development that preserves and enhances the rural and environmentally sensitive qualities of the Northern Planning Area, while providing adequate opportunities for residential growth. Overall, the Plan creates a maximum development potential of 600 dwelling units. While maintaining a single family development character for most of the planning area, the Plan uses a full range of densities, from 0-1 dwelling units per acre (du/ac.) to 8-12 du/ac., to respond to different site conditions and housing needs. In addition, the Plan provides two sites with 8-20 du/ac densities for the development of housing projects for low and very low income families, in support of the City's Housing Element.

The Plan concentrates most development (and the higher densities) on Area A's flat lands, especially in the valley along the northern edge property line. The rural character of the environmentally sensitive ridgeland areas is maintained with larger lots and open space provided by clustered development. The traditional character of Healdsburg's older residential areas will serve as a model for development and site planning in the less environmentally sensitive areas through the establishment of a similar pattern of streets, open space, and development.

*(Figure 5A)*

The Plan preserves hills, vegetation and creeks to maintain the identity to Area A and to create a framework within which development may take place. Natural features such as sensitive ridgeland, steep slopes, creeks, mature stands of vegetation, and valued habitats are protected.

Man-made elements of the Plan, such as roads, parks, trails, and private development, have been planned to enhance the identity of Area A, as well as to complement the Area's natural features. The structure provided by the streets and open space will serve to unify the natural conditions and functional requirements to create a distinctive image and identity for Area A. In addition, development standards and design guidelines in the Plan will ensure that private development maintains a consistent overall character.

Area A contains four subareas that are distinct because of their terrain and location. In response to these differences, the Plan establishes different development characteristics for each area. The four subareas are:

### 5.1.1 NEIGHBORHOOD CENTER

A compact, pedestrian-oriented Neighborhood Center is located in the small, flat valley at the north end of the area. The Neighborhood Center, which has been planned to reflect the form and character of older Healdsburg, organizes development and open spaces within a grid of streets. Area A's higher density designations are located in this subarea.

### 5.1.2 RIDGELANDS

The majority of Area A is included in this subarea, which includes the ridgeland areas and other environmentally sensitive locations identified as constraints to development. In order to respond to environmental constraints and to maintain a rural character, the ridgeland areas have been planned for low densities (predominantly 0-1 du/ac.) with either large lots, or clustered development with variable lot sizes and/or common open space areas.

### 5.1.3 GATEWAY

A relatively small area located at the intersection of Healdsburg Avenue and the main entry road to Area A is planned as a "gateway" into the planning area. The Plan calls for a cluster of higher density development and landscaped setbacks designed to establish a rural character and an image for the area.

#### **5.1.4 NEIGHBORHOOD EXTENSION**

The southwestern corner of the planning area has been planned as an extension of the existing residential neighborhood to the south. A street connection would be made between the two areas and the development densities and character of the new development would reflect that of the existing neighborhood along Area A's southernmost boundary.

Policies regarding these subareas and their characteristics are contained in subsequent chapters. The Land Use and Housing Element discusses development mix and intensity. Design standards contained in the Urban Design Element establish the character of future development in each subarea. The proposed street standards and street hierarchy are described in the Circulation Element. Policies relating to open space can be found in the Open Space Element.

### **5.2 CIRCULATION**

The Plan provides for an integrated system for vehicular and pedestrian movement within Area A, the goal being to promote safe and convenient circulation which creates a cohesive and interconnected neighborhood unit. In addition to internal circulation, the Plan also provides for future street and trail connections with adjacent areas. The primary access to the site will be from Healdsburg Avenue, which provides the only direct and through connection to downtown Healdsburg from Area A. The primary, gateway entry road to Area A will be located about 1000 feet north of the Healdsburg Avenue/Chiquita Drive intersection.

Internal streets within Area A have been laid out to provide access to the entry road which provides a central east-west spine. The entry road is planned to ultimately extend east of the site to connect to the street system in Area B. From this road, two streets extend south through the Ridglands to connect with existing streets and streets will extend north to ultimately connect with the Area C street system. The layout of the internal streets is based on the existing topography, location of fault traces, and boundaries between zoning districts found in the Plan. On the flat lands, the Plan calls for the use of a grid pattern of streets modeled upon the street pattern of historic Healdsburg. In the steeper areas where topography does not accommodate a grid, the streets have been laid out to respect existing topography as much as possible.

The pedestrian system generally parallels the vehicular system, although, wherever possible, the pedestrian/cyclist trail may be separated from the roadway to reduce conflicts, enhance the experience of pedestrians and cyclists, and take advantage of natural features and open space areas. Sidewalks are located within the right-of-way.

The Plan establishes a hierarchy of streets designed according to the functional requirements of the streets, and the surrounding land uses. The Plan is based on the idea that streets play a strong role in determining the character of an area. Consequently, the street standards have been developed to respond to the terrain and character of the different development subareas. Landscaping standards also have been tailored to the character of the subareas, with the more developed areas receiving a more ornamental and regularly spaced planting treatment, and the more rural area landscaping incorporating more native vegetation in a naturalistic pattern.

### **5.3 OPEN SPACE**

The Plan establishes a system of open spaces to meet the recreational needs and enhance the image of the planning area. The open space system is divided into two categories: open space for active and passive uses. Open space for active uses includes areas set aside for parks and trails, and open space areas for passive uses include primarily resource protection areas, including privately held open space. Streets and roads play an important role in the open space system as corridors that link the open space areas into an interconnected network.

The Plan designates 10 acres for a public park located in the northern portion of the Planning Area. Approximately 5.5 acres will be provided for active recreational use in Area A with the remaining acres provided in Area C. The park will be owned and maintained by the City of Healdsburg.

The remainder of the open space area is planned for passive open space use. In order to reduce fiscal impacts to the City related to maintenance of large areas of open space and to preserve sensitive natural resources, the majority of the passive open space will be held in private ownership. Areas with identified environmental constraints, such as the areas of riparian woodland, natural wetlands, and serpentine chaparral will be off-limits to development (unless mitigated for loss) and included within the acreages of large lots, or within publicly or privately owned and maintained open space areas. This acreage includes a steep serpentine slope and a natural wetland depression on the eastern edge of the planning area, and various stream corridors.

## **5.4 PUBLIC FACILITIES AND INFRASTRUCTURE**

To accommodate the level of development planned for Area A, substantial infrastructure improvements and public facilities are required by the Plan, including roads, water and sewer systems, storm drainage, and schools. The Plan describes the necessary improvements required. In addition, off-site improvements necessary to accommodate planned development have been identified. All improvements have been designed based on buildout of the entire Northern Planning Area. The *Implementation Element* of the Plan identifies phasing for proposed improvements. Costs and construction responsibilities are detailed in the Financing Plan.

### **5.4.1 SCHOOLS**

At buildout, the Northern Planning Area is projected to generate as many as 350 elementary students without density bonuses. Although the precise need for a new school has yet to be determined, a school site is planned for the Northern Planning Area. At least 10 acres will be reserved in Area C for the school.

### **5.4.2 STORM WATER AND DRAINAGE FACILITIES**

A detention basin will be constructed in Area A to detain run off from new development and avoid potential downstream flooding of Foss Creek in central Healdsburg. In addition to the detention basin, the Plan includes policies to minimize storm runoff volumes and velocities in order to reduce flooding potential. Among other things, the Plan recommends that open drainage channels be maintained wherever practical and areas of impervious surfaces be limited.

Approximately 10 acres will be required for the detention basin, all of which will be provided in Area A.

### **5.4.3 WATER**

The availability of water and the capacity of existing City lines appears adequate to accommodate the project. Additional storage capacity will be needed at the Gauntlett Reservoir. A new storage reservoir of at least one million gallons will be required before development in Area A exceeds 25% of the projected units.

Those areas above an elevation of 320 feet are above the normal service line and could suffer from inadequate water pressure. The use of individual booster pumps is recommended for the small number of residences that would be located above 320 feet.

### **5.4.4 SEWER**

Sewer service to the planning area will require off-site improvements in addition to the normal installation of sewer mains within the area. The existing trunk main along Healdsburg Avenue does not have enough remaining capacity to accept any new effluent from development in Area A. The Plan recommends the construction of a new trunk line to handle the additional demand.

#### **5.4.5 ELECTRIC**

Primary electrical service will be provided by existing overhead lines along Healdsburg Avenue. At the southern limit of Area A, electric service will be extended north to the entry road. An existing PG&E overhead line that traverses the easterly portion of Area A will be moved to the proposed alignment of a residential street parallel to the line.

#### **5.4.6 ROADS**

In addition to the construction of new roads to serve development within Area A, the Plan will also require road improvements to Healdsburg Avenue including widening to five lanes along the frontage from Dry Creek Road to the Entry Road and to three lanes from the Entry Road north to the Area A boundaries. Installation of traffic signals at the Area A entry road and at Chiquita Road will be required. With these improvements a level of service "C" or better is anticipated at these intersections.

## **6. LAND USE ELEMENT**

# 6 LAND USE ELEMENT

This chapter of the Specific Plan translates the Goals (Chapter 4) into specific land use policies, objectives and standards. It specifies the future distribution, location, and intensity of land uses within Area A. In addition to the Specific Plan policies and objectives, all Land Use Goals, Policies and Implementation Programs contained within the Healdsburg's General Plan (Part II, Section I, pp. 19-23) are applicable to Area A with noted exceptions. Goals, policies and implementation measures found in the City of Healdsburg - Housing Element are also applicable to Area A, especially as they relate to achieving the City's Quantified Objectives for the construction of new housing. (also see Chapter 7, Housing Element).

The Land Use Plan describes the physical pattern of development permitted in Area A. Road alignments, and lot lines where shown, are approximate. The Plan is an expression of development considerations that:

- respond to biological, geotechnical and other environmental constraints;
- recognize City, landowner and citizen preferences;
- provide a range of housing opportunities;
- maintain the character of small town Healdsburg and its rural surroundings.

(Figure 6A)

## 6.1 LAND USE CONCEPT

The land use plan is based on two concepts:

1. The proposed development pattern should be derived from and respond to both the natural character of the site and the social and cultural character of the City.
2. Land use patterns should enhance the quality of life for planning area residents.

In response to the first concept the Plan identifies a framework of natural elements including creeks and drainage ways, steep slopes, ridgeline areas, and sensitive habitat areas, which are incorporated into an open space system that provides the basic structure for planning area land uses. The natural open space framework of the Plan is supplemented with a framework of man-made elements, such as roads, parks, trails, and public facilities.

The framework of man-made elements has been developed to respond to the site's functional requirements and to work with the site's natural features. Together the roadway and open space systems provide an integrated structure which unifies the area and establishes the basic form and identity for planning area development.

In order to build on the traditional character of Healdsburg's older areas, the Plan establishes a similar pattern of streets and parcelization in Area A, particularly where the site is flat. Overall, the Plan concentrates the majority of the development and higher densities in the flat areas of the site, especially the valley at the north. In environmentally sensitive areas, the site's rural character is maintained by restricting development to large lots or clustered development. The Plan includes siting and development standards that prohibit development in environmentally sensitive locations.

Area A establishes four subareas that are distinct because of their terrain and location. In response to these differences, the Plan establishes development subareas as well, each with its own character. The four development subareas include:

### 6.1.1 THE NEIGHBORHOOD CENTER

The valley in northern Area A (and southern Area C) offers an opportunity to create a compact and pedestrian-oriented neighborhood that emulates the pattern of development in the older sections of Healdsburg. While

#### **6.1.4 NEIGHBORHOOD EXTENSION**

Development in the southwestern corner of Area A has been planned as an extension of the existing residential neighborhood to the south. A street connection would be made between the two areas at Rosewood Drive, and the development densities and character of the new development would reflect that of the existing neighborhood along Area A's southernmost boundary.

### **6.2 LAND USE CATEGORIES**

The Land Use Plan contains the following land use categories:

#### **Very Low Density Residential**

This category will permit either detached single-family dwelling units at densities of 0-1 dwellings per acre (du/ac), or any type of dwelling units using an optional cluster overlay zone at densities up to 1.3 du/ac. Overlaid on this category was a map of areas with sensitive environmental resources such as vegetation, steep slopes, ridgelines, etc. which are protected by City policies found in the General Plan and which have been identified in the environmental assessment of Area A as sensitive and requiring protection (see Figure 11B). Within the overlay area, the siting of new development will be strictly controlled to avoid disturbance or degradation of these resources through enforcement of the siting, grading and design standards contained in the Community Design Element of this Plan. The densities for the residential land uses described below are based on gross acreages which are generally measured from the center of roads.

#### **Medium Density Residential**

This category will permit detached single-family dwelling units at densities of 3-6 du/ac.

#### **Medium-High Density Residential**

This category will permit detached single-family dwelling units at densities of 6-8 du/ac.

#### **High Density Residential**

This category permits multiple dwelling units on the same lot (attached or detached) at densities of 8-12 du/ac.

#### **Public**

This category contains publicly-owned lands designated for the development of schools, parks and a detention basin that will be publicly maintained as part of a storm water management system for the Northern Planning Area for active recreational uses.

In all the above residential land use categories, increased densities are allowable under provisions of the affordable housing (AH-1 and AH-2) overlay zoning districts and pursuant to State law in regard to density bonus. Area A includes two sites which will be pre-zoned as AH-1 overlay zones. This overlay district permits high density housing (multiple attached or detached units on the same lot) up to 20 dwellings per acre (with State density bonus), along with other development standard incentives, for the purpose of allowing housing opportunities for low and very low income families and individuals.

### **6.3 LAND USE PROGRAM**

The Land Use Program for Area A calls for a mixture of residential uses, ranging from large lot, single-family detached units to multi-family units, supplemented with appropriate support facilities and open space. Ninety-three percent (93%) of the Area or approximately 220 acres is designated for residential development. Of this total, approximately 6% (13.4 acres) is designated for high density residential development.

Based on the density ranges set forth in the Plan, Area A could accommodate between 441 and 819 dwelling units under the base zoning. However, based on subarea development limits as shown in Figure 12B of the Plan, the projected development potential for Area A will be no more than 600 dwelling units. There are 17 existing homes

topography separates the Neighborhood Center from existing neighborhoods to the south, the Plan would reflect Healdsburg's distinct pattern of development at the north end of town.

Streets, paths and open space create the organizing framework for the Neighborhood Center. A grid pattern of streets knits the Neighborhood Center together and follows the traditional development pattern in downtown Healdsburg. The grid configuration has also been designed to respond to seismic faults (i.e., streets have been aligned along fault traces), and, through the axial alignment and orientation of streets, provides frequent physical and visual connections down streets to the wooded hills and other natural features in Area A.

Active and passive open spaces provide opportunities for meeting, playing, gathering or resting. A park and school will provide a site for civic activity. Creeks through the area will be maintained in open channels wherever practical, and will be enhanced with landscaping to add visual interest and to accommodate pedestrian pathways and connections to other parts of Area A.

Guidelines for architecture and landscaping also ensure the creation of a physical environment reminiscent of historic Healdsburg. The arrangement and design of residences and yards encourage walking, informal meeting and are based on buildings and other architectural precedents found in the historic areas of Healdsburg.

Because of the abundance of sensitive environmental features found in Area A (particularly steep slopes and vegetation) especially along Healdsburg Avenue, little flat land suitable for commercial development was available that was visible and accessible to vehicles.

For these reasons, no commercial land has been designated in Area A. However, suitable land for this use may be found in Area C, a location that can serve the entire Northern Planning Area, including Area A. Therefore, some limited neighborhood commercial uses to provide convenience shopping within walking or bicycling distance of residents in the Northern Planning Area, including Area A, should be considered in the flat portion of Area C once a Specific Plan is developed for that area.

For similar reasons, a transit stop has not been identified in Area A. With easy access and visibility along Healdsburg Avenue, Area C should provide the location for the transit stop.

### **6.1.2 RIDGELANDS**

The Plan retains the rural character of surrounding ridgelines and intervening swales by: 1) placing them in a zoning classification that requires large lots or clustering of lots or housing units to maintain open space areas, and 2) restricting location of new development based on siting standards to avoid certain environmentally sensitive locations. These requirements are necessary to minimize disturbance or degradation of important habitats, geological hazards, steep slopes, poor access, or areas of scenic value. The siting standards will ensure that structures will be carefully located with regard to these environmental constraints either at a distance sufficient to protect natural resources (as outlined by setback requirements or development prohibitions for prescribed sensitive areas) or below ridgelines to preserve views and scenic resources (as described in the General Plan).

Roads in these areas will have a rural character by maintaining narrow cross-sections, by curving with the terrain, by appropriately landscaped, and by avoiding important natural features.

### **6.1.3 GATEWAY**

The Gateway is planned as a cluster of development at the intersection of Healdsburg Avenue and Area A's primary east-west collector, or entry road. The intent of this designation is to create a distinctive entry statement that will establish an image and identity for the entire Northern Planning Area. Here, gentle-to-moderate slopes provide an opportunity to create a small pocket of intensive development. Landscaping and setback requirements are intended to create a rural character and scenic entry for Area A.

## **6.4 DEVELOPMENT CHARACTER**

### **Policy**

- Provide a pattern and intensity of land use that reflects historical patterns and at the same time respects natural constraints and conditions.

This policy restates Land Use Goal C in the General Plan. The land use pattern in Area A contributes to the achievement of this policy by creating a land use pattern that reinforces the traditional town character of Healdsburg and its rural setting, while protecting Area A's sensitive natural features. The Plan establishes siting and development standards and encourages clustering of development to protect natural elements such as ridgelines, steep hillsides, valuable vegetation, and creeks. Together, these features form an open space framework that preserves a rural character for Area A. All intensive development and active recreation uses are located in areas without significant environmental constraints.

The land use pattern extends the character of historic Healdsburg and surrounding rural lands into the Northern Planning Area through a pattern and intensity of development and the creation of private and public open space. To avoid the loss of Healdsburg's unique sense-of-place, the land use pattern avoids typical suburban-type development that fails to respond to Healdsburg's historic development patterns or to take advantage of place-making natural features. The land use pattern is coordinated with a hierarchy of streets, which establishes a grid pattern in the flatter, more developable areas and curvilinear alignments that respond to the terrain in more sensitive areas. The Plan introduces the grid pattern of streets to create a sense of continuity with the layout in older portions of the City. To further reinforce the connection, the size and spacing of the blocks in Area A is based on those in downtown Healdsburg and similar historic towns in California which are centered around town squares.

*(Figure 6B)*

The Plan provides street design standards which will achieve the appropriate aesthetic character for Area A based on the function of the street for pedestrian and vehicular movement and the specific environmental constraints affecting the street alignment. In general, street widths have been minimized and improvement standards (curbs, lighting, landscaping, etc.) aim for a more rural character.

Because of their relevance to Area A, the first three land use objectives reiterate General Plan Land Use Policies C.1-C.3. The remaining objectives translate General Plan policies into objectives specifically pertaining to Area A.

### **Objectives**

- Only very low- and low-intensity land uses shall be allowed in areas characterized by steep slopes, environmental hazards and scenic ridgelines and hillsides.
- Intensive urban development shall be allowed only in areas that are relatively free of topographic, geologic, and environmental limitations.
- The City may grant a density bonuses for residential projects. These are described in Chapter 7 - Housing Element.
- A compact Neighborhood Center should be created on lands with few environmental constraints, around the Area A/Area C border, with higher-density housing and park.
- The rural character of Ridgeland areas should be maintained by preserving ridgelines as open space through clustering development or restricting to large lot development.
- The residential development pattern and densities which exist south of Area A will be extended into the immediately adjacent areas in the southwest corner of the Area A. The hills and existing development will define the northern extent of this development pattern.

in Area A.

In addition, Area A contains approximately 5.5 acres for a park. Land required for the detention basin will total approximately 10 acres. Acreages given here for the park and detention basin are preliminary since a portion of the detention basin is anticipated to include joint use as public park and/or mitigation site for wetland or riparian impacts. Final determination of acreage needed for these public use lands in Area A will be based on updated hydrological analysis setting design parameters and incorporating such joint use within the detention basin to the extent practical or needed. Approximately 9 acres will be taken up in primary road system.

The following table describes in quantitative terms the Area A Land Use Program. It summarizes:

- The total area in each land use category;
- The range of density and dwelling units possible (if applicable);
- The number of units anticipated by the Plan.

### LAND USE PROGRAM

	Area	Density Range	Unit Range	Units Anticipated
	acres	du/ac	du	du
<b>Public</b>				
Park	5.5	n/a	n/a	n/a
Detention Basin	10	n/a	n/a	n/a
<b>High Density Residential</b>				
with AH-I Overlay Zone	2	8-12	16-24	24
	11.4	16-20	182-228	105
<b>Medium-High Density Res.</b>				
	20.9	6-8	125-167	135
<b>Medium Density Residential</b>				
	33.77	3-6	101-202	161
<b>Very Low Density Res.</b>				
Existing Residences	152.12	0-1.3	0-198	158
	n/a	n/a	n/a	17
<b>Total</b>	<b>235.69</b>	<b>n/a</b>	<b>441-819</b>	<b>600*</b>

**Notes:**

\* Total anticipated units assumes all property owners in the Ridglands using the Cluster Development Overlay Zone which allows densities up to 1.3/du/ac, and 105 units of low and very low income housing using a proposed AH-I Overlay Zone in the areas shown on the zoning designations map. The number of total anticipated units also takes into account preliminary development plans submitted to the City by two property owners, ZAM enterprises and Vinecrest Properties, in which the proposed number of housing units is lower than would otherwise be possible under the proposed density designations and zoning. To ensure that these assumptions are not exceeded, the Specific Plan limits the number of dwelling units that can be constructed in the subareas of Area A as shown in Figure 12B.

- A gateway entrance into Area A will be established at the intersection of Healdsburg Avenue with the primary east-west collector. The intent of the gateway area will be to create a distinctive image or identity for Area A through landscaping and development setbacks.
- The site's unique and/or sensitive natural features will be preserved as open space, including visually-sensitive ridgetops, steep slopes (generally over 20%), creeks, and areas with high-value habitat.
- A 30-foot minimum landscaped buffer will be maintained to preserve a rural character of the road and avoid siting of major obstructions within scenic view corridors. Also, a landscaped setback should be provided on the Entry Road to maintain a rural character for that road. Setbacks can be maintained as either public or private open space. Natural features such as creeks, ponds and woodlands, should be integrated into these road corridors where possible. Create natural-looking, landscaped drainage swales along designated streets to further enhance the rural character of Area A.

## **6.5 CONSERVATION AND PUBLIC SAFETY**

### **Policies**

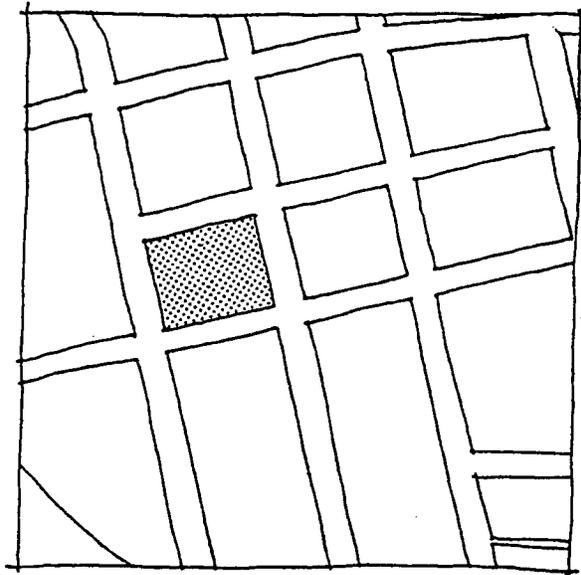
- A land use pattern will be created that ensures public health, safety and welfare.
- A land use pattern will be created that ensures the conservation of natural and historic man-made resources.

### **Objective**

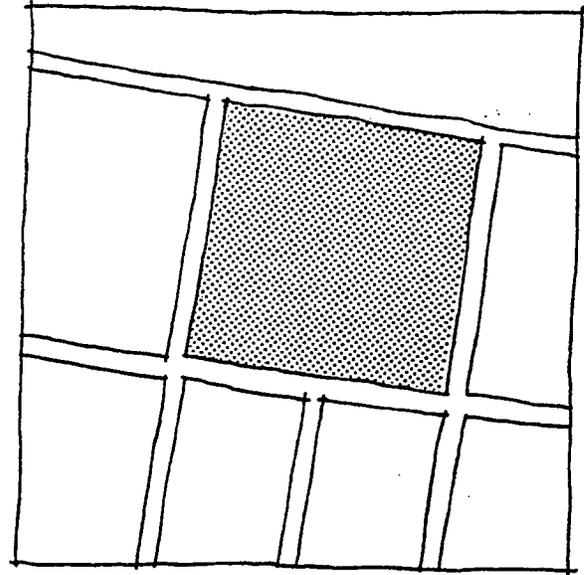
- Development will be restricted in those areas that pose a threat to public health, safety, welfare or environmental protection. Earthquake faults and other geologic conditions pose risks to the public. Development in proximity to streams or major vegetative stands can result in increased erosion, degradation of wildlife habitats and diminution of visual quality.

### **Standards**

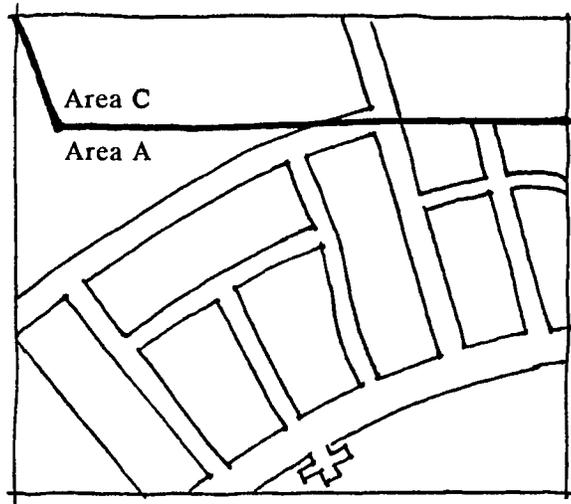
- a. Geotechnical Concerns
  - i.) Avoid siting structures in areas prone to flooding. If residential structures are sited within the 100-year floodplain, finished floor elevations must be one foot above the water level for a 100-year storm.
  - ii.) Minimize grading for all development. In general, structures should be designed in response to the topographical and geotechnical conditions. In hillside areas in particular, foundations designed for sloping sites should be used rather than recontouring the site to accommodate flat land construction techniques. In areas of steep topography, structures should be sited near the street to minimize required grading.
  - iii.) No structure for human occupancy shall be located within 50' of an active seismic fault trace, or as approved by the City's geotechnical consultant.
  - iv.) No structures, roads or driveways shall be sited within the following areas unless detailed feasibility and design-level geotechnical investigations indicate that development can be safely undertaken and/or mitigation measures can be implemented which will reduce impacts to a level of insignificance:
    - slopes of over 30%
    - areas underlain by active or dormant landslides
    - flood plain areas underlain by alluvium
  - v.) No structure shall be located on slopes of between 20 to 30%, where this location is downslope of colluvium or dormant landslides on slopes over 30%, unless detailed feasibility and design-level geotechnical investigations indicate that development can be safely undertaken and/or mitigation measures can be implemented which will reduce impacts to a level of insignificance.



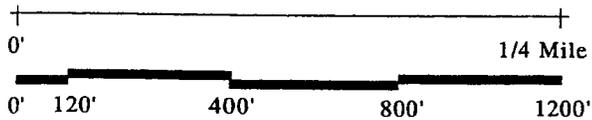
HEALDSBURG



SONOMA



NORTHERN PLANNING AREA



Note: Distances are approximate. Road widths (right-of-way) are also approximate and width conventions may have been used.

Source: U.S.G.S.

NORTHERN CALIFORNIA TOWN GRIDS  
 NORTHERN HEALDSBURG SPECIFIC PLAN  
 AREA A

Wallace Roberts & Todd  
 Jones & Stokes Associates

Figure 6B

## **7. HOUSING ELEMENT**

- vi.) No structure shall be located on slopes of 10-30%, where underlain by highly expansive soils, areas of fill or within 100' of incised stream channels, unless detailed feasibility and design-level geotechnical investigations are undertaken and required engineered design mitigations performed.
  - vii.) Graded areas for development, excepting driveways, shall not exceed 12,000 square feet in areas with predominant slopes exceeding 10%.
  - viii.) Areas requiring detailed geo-technical studies can be found on Figure 11B in the category of "Landslide."
- b. Biological Resources.
- i.) Where practical, use natural drainage ways, rather than channelized or piped drainage structures. Drainage ways should be revegetated with native trees along the top of banks to achieve a natural appearance and improve habitat conditions. In-channel revegetation may only be appropriate where it will not hinder long-term maintenance and drainage capacity
  - ii.) Avoid development and potentially destructive activities in areas with high-value habitat including:
    - serpentine chaparral
    - riparian woodland
    - natural wetlands

Exceptions may be granted only where an owner's reasonable beneficial use of the land cannot be otherwise provided or where adequate mitigation is proposed to reduce impact to a level of less than significant.
  - iii.) Minimize development and potentially destructive activities in areas covered by moderate-value habitats including:
    - oak woodland/forest
    - mixed evergreen forest
- c. Aesthetic Factors
- i.) Habitable structures shall be set back at least 170 feet from Healdsburg Avenue or an acceptable method of sound control shall be utilized to prevent noise levels above 60 dB for residents and to maintain uninterrupted open space along this scenic route.  
Garages and yards shall be allowed within the setback.
  - ii.) A 30-foot minimum buffer shall be established along the east side of Healdsburg Avenue in Area A and shall be landscaped in accordance with Chapter 9, Section 9.1.3 to screen development from Healdsburg Avenue and buffer noise.
  - iii.) In order to integrate the planning area with surrounding areas, existing development patterns in areas abutting Area A should be extended into suitable areas within Area A.
  - iv.) Avoid siting structures in locations where the structure would obstruct scenic views or appear to extend above an identified scenic ridgetop when seen from the designated scenic routes. Projects with potential scenic impacts will be required to submit graphic sections or computer-generated photo montages drawn from Healdsburg Avenue and from Area A's main entry road through the parcel in question, representing common views of the parcel from these scenic routes. The graphic depiction of grading, the location and massing of the structure and associated landscaping can then be used to adjust the project design to minimize the visual impact.
- d. Cultural Features
- Avoid development on sites containing structures or features of potential historic or archaeological value that may be degraded or destroyed by the construction activity. Adaptive re-use or restoration is encouraged where economically feasible. If avoidance or re-use is demonstrated to be infeasible or unreasonable, a qualified archeologist shall analyze the site in question as to its actual significance and develop appropriate mitigation measures, that shall then be implemented by the developer.

# 7 HOUSING ELEMENT

The policies and objectives set forth in this element apply specifically to residential land uses in the Specific Plan area. Housing Goals, Policies and Implementation Programs contained within the General Plan Land Use Element also apply to the North Area as well. Several of these goals and policies are restated below to emphasize their importance to guide the proper development of Area A.

## **Policies**

- Provide a mixture of housing densities and types for all economic segments of the community, while maintaining Healdsburg's single-family character.
- Ensure the provision of adequate services to support existing and future residential development.
- Conserve energy and water resources through the design of new residential neighborhoods and structures.

These policies are also reflected in the City's Housing Element, adopted in 1991 and subsequently amended in 1993 and 1994. The Housing Element contains detailed policies and implementation measures which will apply to Area A.

The predominant land use in the North Area will be housing. Since few dwelling units exist today in the planning area, virtually all of the units anticipated in the Specific Plan will be newly constructed rather than rehabilitated or redeveloped. As such, Area A provides a unique opportunity to further the achievement of the City's Quantified Objectives for construction of new housing in the community.

The variety of housing types and densities provided in the Plan contributes to neighborhood diversity and provides incentives for construction of affordable housing. Housing, especially affordable housing, is in short supply and areas suited for new residences are limited. The Northern Planning Area is seen as a primary area for accommodating the growth in the City's housing stock.

The intent of the Plan is to facilitate the provision of a full range of housing types and establish appropriate conditions for creating cohesive residential neighborhoods. Consistent with the Housing Element, an Inclusionary Housing Program will apply to all projects of ten (10) units or more located within Area A. Details of the Inclusionary program, including the provision of units on- and off-site, dedication of and or payment of in-lieu fees are found in the Housing Element and implementing ordinances.

Energy and water resources may be more limited and costly in the future and must be conserved through neighborhood and residential design. The Specific Plan provides for a pedestrian-oriented neighborhood with sidewalks, pathways and a trail system to encourage walking and bicycling. Large areas of natural vegetation will be preserved under provisions of the Plan, thus reducing the potential for water-consuming ornamental landscaping. In addition, small lots and higher densities in the Neighborhood Center will reduce water use related to residential landscaping.

## **Objectives**

The following specific implementation programs will ensure that housing policies and objectives are carried out as set forth in the Plan.

- Provide high- and medium-high density housing for fee simple purchase, condominium ownership or rental in those areas where compatible with topography, natural features and existing neighborhoods.
- New high density shall be designed in accordance with design guidelines related to the maintenance and continuation of Healdsburg's single-family quality (See General Plan Scenic Resources and Urban Design Element).

**8. OPEN SPACE  
ELEMENT**

- Allow appropriate densities in areas exhibiting environmental and/or aesthetic constraints and only when proposed development is consistent with environmental or aesthetic goals (See General Plan Scenic Resources and Urban Design Element).
- Encourage new dwelling unit types which meet City specified density and design criteria, while providing a mixture of densities, and ownership and rental opportunities.
- Encourage energy efficiency as provided for in the General Plan (Housing Element, Goal E).
- Promote water conservation as provided for in the General Plan (Public Facilities and Services Element, Goal A, Policy 6).

### **Implementing Programs**

The following specific implementation programs will ensure that housing policies and objectives are carried out as set forth in the Plan.

- Two sites totalling approximately 11.4 acres of the Affordable Housing Overlay Zoning District 1 (AH-1) will be designated within the Neighborhood Center subarea of Area A. As shown in the land use plan (See Figure 6A), one site of approximately 9 acres is located on the south side of the Entry Road approximately 220 yards east of Healdsburg Avenue. The other site of approximately 2.4 acres is located on the north side of the Entry Road at the far northeast corner of Area A. The provision of these two sites for affordable housing developed under the provisions of the AH-1 overlay zoning district, on property currently owned by ZAM Enterprises (Assessor Parcel Nos. 091-040-05, 090-040-28, and 091-040-44), will satisfy the requirements of the City's proposed Inclusionary Housing Program requirements for the developer of this property. A copy of the AH-1 ordinance is found in the Appendix of this Specific Plan. Key features of the AH-1 Overlay District include:
  - The AH-1 Overlay district permits a maximum residential development density of 16 units per acre, net, in addition to any state-mandated density bonuses.
  - Development on parcels zoned as AH-1 may develop under the requirements of the "base" Specific Plan designation, or under the provisions of the AH-1 Ordinance.
  - All dwellings constructed pursuant under the AH-1 Overlay District shall be made affordable to low and very low income families and individuals. Long-term guarantees shall be made to the City of Healdsburg guaranteeing that all units constructed under the provisions of AH-1 shall be made available to low and very low income groups as specified in the Healdsburg Housing Element.
  - Special incentives for the construction of affordable housing under the provisions of the AH-1 are found within the text of the AH-1 Overlay District.
- In addition to the AH-1 Overlay District, all residential property contained within the Area A Specific Plan may take advantage of the AH-2 Affordable Housing Overlay District. The text of the AH-2 Affordable Housing Overlay District is found in the Specific Plan Appendix. Key provisions of the AH-2 Overlay District are as follows:
  - The AH-2 Overlay District shall not apply to any industrially- or agriculturally-zoned property, or any parcels zoned AH-1.
  - The AH-2 Affordable Housing Overlay District is intended to permit and encourage small increments of affordable housing integrated into the surrounding neighborhood.
  - Similar to the AH-1 Overlay District, the AH-2 Overlay District shall be affordable to low and very low income families and shall remain affordable to such groups for a time span mandated by the Housing Element.
  - Enforce the City's inclusionary policy for all new residential construction, including the provision that at least ten (10) percent of all new projects consisting of at least 10 dwelling units be made affordable as provided for in the Housing Element and all implementing ordinances.

# 8 OPEN SPACE ELEMENT

The intent of the Open Space Element is to preserve and manage the planning area's natural resources and open space lands for the benefit of both planning area residents and the wider community and its visitors. A well-planned and effective open space program provides an important visual asset which not only enhances community character, but also increases land and housing values in the area. Similarly, open space is an important recreational asset which improves the quality of life for area residents and generates community pride. By maintaining environmentally constrained areas as open space, the Plan avoids potential costs associated with installation of infrastructure through constrained areas and with clean up and repair when development is sited in hazardous areas subject to landslides, flooding, or hazards.

The Open Space Element describes the open space framework established by the Plan and associated open space and conservation policies. Open space, in this context, refers to land which is undeveloped, but does not mean the land must remain unused nor does the land have to be in public ownership. The Plan establishes a system of open space and conservation areas that are in public and private ownership and accommodate passive and active uses. The function or role of the open space areas in Area A differs from one another, with the areas falling into three general and sometimes overlapping categories:

- Open space for the preservation of natural resources including, but not limited to habitat areas required for the preservation of plant and animal species (oak woodlands, natural wetlands, riparian woodland, serpentine chaparral and evergreen forest), and creeks, ponds and drainage ways.
- Open space for public health and safety, including but not limited to, areas which require special management or regulation because of hazardous or special conditions such as:
  - earthquake fault zones,
  - unstable soil and slope areas,
  - flood plains,
  - areas with high potential fire hazard.

Also included in this category are lands required for infrastructure to serve the site such as detention basins and rights-of-way for streets and utilities.

- Open space for outdoor recreation, including but not limited to the community park area; areas of outstanding scenic, historic, or cultural value; and corridors which serve as links between recreation and open space areas, including, bicycle paths, pedestrian paths and trails, and scenic roadway corridors.

Although the individual components of the open space system fulfill different functions, together they provide a comprehensive interconnected open space framework for development within Area A. The framework consists of both publicly- and privately-owned open spaces. The community park, trail system, and street rights-of-way represent the publicly-owned components of the open space system available for active and passive recreation.

The privately-owned components consist of resource protection areas and are recommended for passive use only.

*(Figure 8A)*

## 8.1 PUBLIC PARKS AND TRAILS

This section sets forth policies and objectives relating to public parks, paths and trails. Related policies contained in the Cultural and Recreational Resources section of the Healdsburg General Plan (Part II, Section VI, pp. 49-54) are

also applicable to Area A. One discrepancy between the General Plan and the Specific Plan should be noted, however. In the General Plan, Area A is shown as the location for a potential view park and a neighborhood park, but neither one has been established by the Specific Plan.

The Specific Plan does establish a site for a community park on the boundary between Area A and Area B. Shared between Areas A and C, the 10-acre community park site is adjacent to the proposed elementary school site.

The creation of a view park is encouraged by the Specific Plan, but it is felt that the appropriate location is on the ridgeline east of Area A, in Area B. Given the site topography and the distribution of population within the planning area, no appropriate site was identified for a neighborhood park within Area A. Sites that are level enough to accommodate an active park and close enough to concentrations of housing are generally too close to the community park and would provide a duplication of services.

### **Policies**

- Provide adequate park and recreation facilities for north Healdsburg, suited to the needs of area residents.
- Establish a convenient network of pedestrian paths and trails and bicycle routes connecting the planning area to the surrounding community.
- Encourage creation of a public view park on Healdsburg Ridge which is connected to the area's pedestrian trail system.
- Encourage the preservation of privately held open space to provide for passive uses and to contribute to Area A's rural sense-of-place.

### **Objectives**

- Provide sufficient park acreage in the northern planning area to meet the Healdsburg General Plan goal of 5.0 acres of parkland per 1,000 population at each stage of development.
- Secure open spaces which accommodate active recreational needs and enhance the image of a neighborhood. Healdsburg's northern areas presently lie at a considerable distance from most park and recreation facilities, and the City is presently deficient in park land per resident given State and national standards. Consequently, a community park is proposed by the General Plan on the boundary between Area A and Area C.
- Maintain visual and physical access to open space. Open space is a common characteristic of Healdsburg which provides a frequent reminder of the City's rural scale and setting. Pedestrian paths and bicycle trails provide access to natural and scenic features, and reduce reliance on the automobile for movement throughout the area.
- Recognize that Healdsburg Ridge, with its exceptional views of the valley, provides an excellent opportunity for a view park as recommended by the General Plan. It is anticipated from preliminary planning for Area B that the ridgeline east of Area A will be maintained as open space with public trail access. The higher elevation of the ridge in Area B combined with its anticipated open space status makes it the preferred location for the development of a view park.
- Recognize that the potential for public ownership and maintenance of open space areas is limited given available City funds and resources necessary to cover park maintenance, policing and liability. The use of deed restrictions or conservation easements to provide additional open space areas for the enjoyment and use of residents is considered an important objective.

### **Standards**

- a. Provide 10 acres of suitable land for a community park in the Northern Planning Area, the first section of the park acreage provided in Area A, at a location contiguous with Area C. Parkland shall be acquired, either in Area A, Area C, or both Areas A and C, commensurate to the number of units constructed in Area A at each stage

of development based on maintenance of the general plan standard of 5.0 acres of parkland per 1,000 population. Any one or a combination of the following measures may be used to comply with the standard:

- i.) Additional acres in Area C may be acquired concurrent with acquisition of acreage in Area A to construct ball field(s) and other interim park facilities.
- ii.) Additional acres in the northern portion of Area A may be acquired to construct ball field(s) and other interim park facilities.
- iii.) Secondary play fields and other park amenities may be located within the detention basin to free up more land for active recreational fields in the park.
- iv.) Park development funds for additional land acquisition may be used in combination with community organizations and individuals contributing labor and/or funding resources for park development.

The park shall accommodate active recreational uses, including baseball, softball and soccer fields. The minimum facility standards are:

- softball/little league baseball
    - 300 feet minimum distance to left center and right field fences
  - soccer
    - minimum dimensions - 195 feet X 330 feet
    - maximum dimensions - 225 feet X 360 feet
- b. Encourage the sharing of facilities, such as playfields, between the Community Park and School by locating the Park adjacent to the School.
  - c. Provide a convenient, all-weather network of paths and trails through parks, along streets and roads, and along creeks and ridgetops (See Circulation Element).
  - d. Provide trail access to existing or proposed trails adjoining Area A, anticipating trail easements in Areas B & C.
  - f. Provide landscaped setbacks along streets and utility easements to accommodate trails and provide for easy maintenance.
  - g. Design public pedestrian paths and bicycle trails that do not compromise the security and privacy of residents.
  - h. Design public paths to minimize impacts on sensitive high-value habitats.
  - i. Recognize the limited mobility of some residents and visitors in the design of paths and trails; assure handicap accessibility to paths.
  - j. Encourage development of a trail along the western side of the Healdsburg Ridge (i.e., through Area B) connecting the Neighborhood Center with a possible view park.
  - k. Encourage the private donation of land and/or conservation easements to nonprofit land trusts and open space preservation groups. Fund on-going maintenance by these organizations via a real estate transfer tax applicable to properties within the Northern Planning Area.

## **8.2 ENVIRONMENTAL PROTECTION AND RESOURCE CONSERVATION**

This section establishes policies, objectives and standards to protect valuable habitats and other natural resources by conserving open space. It restates and reinforces the goals and policies contained in the General Plan's Natural Resources Element (Part II, Section VII, pp. 55-58). Siting standards developed to protect environmental resources are contained in the Urban Design Element of this Plan.

### **Policies**

- Conserve valuable habitats and other natural resources.

## **Objectives**

- Maintain Area A's natural features to establish a rural character and contribute to the area's ecological health. These natural features also provide an opportunity to create an unique, natural setting for residents.

## **Standards**

- a. Preserve high-value habitats, specifically riparian woodlands, natural wetlands, and serpentine chaparral, by dedicating such areas as public or private open space and/or by restricting public and private development and activities through implementation of siting criteria and design standards. Such areas to be off-limits to development, unless adequate mitigation is provided to offset any loss in acreage or value to reduce impacts to less than significant, are shown in Figure 8B.
- b. Minimize grading of slopes over 10% to reduce risk of erosion and conserve vegetation and topsoil.
- c. Encourage landscaping with native, drought-resistant plants, particularly when replanting disturbed areas which are to remain as open space.
- d. Minimize rerouting of drainageways where mature riparian vegetation exists. Drainageways that have been rerouted or which are without mature riparian vegetation should be revegetated with native plant species. However, revegetation should consist of planting native trees or shrubs outside the channel rather than riparian species within the channel to maintain drainage capacity and minimize channel maintenance needs.
- e. Where possible, incorporate creeks within public open space and align roads along the creek setbacks to make the creeks a common experience for residents and visitors, and to facilitate maintenance.
- f. Avoid siting any buildings or improvements within existing riparian woodlands (as defined and shown in the EIR). Where buildings or improvements must encroach upon existing riparian woodlands, provide measures to enhance and protect an equal amount of area or more of riparian habitat in the same area, such as replanting of areas lacking significant vegetation or construction of drainage swales where riparian vegetation can be established pursuant to a mitigation plan. Along drainageways without significant riparian vegetation, set back buildings at least 25 feet, and revegetate with native trees and shrubs.
- g. Configure or cluster lots, roads, driveways, building sites, fences and other improvements to minimize removal of oak woodlands and heritage trees. A qualified arborist should work with the design engineers, planners, architects, and developers during different stages of design (e.g. tentative subdivision map stage, building permit stage) to maximize preservation of oak woodlands and heritage trees.
- h. Replace heritage trees to be removed by planting locally native species at a ratio of one seedling or five rooted oak acorns planted for every inch of the diameter at breast height (dbh) of trees removed. Irrigate and maintain trees for a period of at least 3 years. Replacement trees shall be monitored by a qualified arborist for a period of 3-5 years to ensure that seedlings establish successfully. Plants that do not successfully establish should be replaced and monitored for 3 years from the replanting date. Replacement trees should be planted in the same environmental conditions in which they naturally occur, i.e. slope, soil, water regime, etc.
- i. Protect heritage trees and native oaks from construction and landscape impacts. In general, protected preserved trees by fencing the root zone (i.e. defined as 1.5 times the radius between the trunk and drip line) and keeping all grading and construction activity outside this zone. Specific tree protection measures should be proposed by qualified arborists where projects will involve heritage trees and native oaks, with periodic monitoring to ensure that such measures are actually implemented. The developer will be required to provide information to purchasers of property where oaks and heritage trees have been preserved to discourage artificial irrigation and inappropriate landscaping within the driplines of native heritage trees in which long-term health and viability could be adversely affected.

- j. Areas where native trees or shrubs are preserved, whether in private or public open space, may be selectively thinned, limbed up and pruned, to remove dead wood and fuel ladder conditions, as needed to provide fuel breaks and to decrease the risk of wildfire occurrence or spread in the vicinity of structures, improvements or roads.
- k. The location of stockpiling and vehicle staging areas as needed for construction will be shown on grading plans and kept out of environmentally sensitive areas.

## **9. CIRCULATION ELEMENT**

# 9 CIRCULATION ELEMENT

The Circulation Element describes the components of the system for pedestrian and vehicular movement within Area A. The guiding principles of the Circulation Element are to provide a safe and convenient circulation network that contributes to the creation of a coherent, interconnected neighborhood. Policies relating to the Circulation Element are organized into three sections:

- Vehicular Circulation and Parking
- Pedestrian and Bicycle Circulation
- Design Standards

## 9.1 VEHICULAR CIRCULATION AND PARKING

The policies, objectives and design standards in this Specific Plan have been formulated with the intent of establishing a distinctive system tailored specifically to accommodate vehicular movement and parking in Area A. The circulation system has been designed with full consideration of the development proposed for the rest of the Northern Planning Area (i.e., Areas B and C). Design standards for recommended on- and off-site roadway improvements are based on buildout of the current General Plan, including the Northern Planning Area. Recommended off-site circulation system improvements are based on maintaining a level of service of "C" or better for affected roadway intersections and segments during the primary period of Area A buildout and concurrent City-wide growth.

Goals and policies found in the Transportation section of the Healdsburg General Plan (Part II, Section IV, pp. 37-42) are also applicable to Area A, except for City Street Standards (Policy A.2 and illustrated on page 18) which would be superseded by the standards provided in this Element.

### 9.1.1 NETWORK OF INTERCONNECTING STREETS AND ROADS

#### **Policies**

- Establish a network of streets in Area A that facilitates safe, convenient and efficient movement within and through the entire Northern Planning Area.
- Create an interconnected street system linking the northern and southern parts of Area A, and providing multiple connections to Areas B and C and adjacent neighborhoods to the south.
- Establish a direct connection to Healdsburg Avenue that will serve as the primary gateway entrance to Area A. A second access will be provided through Area C.
- Establish indirect connections between Healdsburg Avenue and existing local streets including Rosewood Drive, Poppy Hill Drive and March Avenue/Highland Circle/University Street (via Area B).
- Incorporate the recommendations in the "North Healdsburg Area Traffic Study, March 1990" report relating to the phasing and construction of improvements to Healdsburg Avenue.

Healdsburg Avenue, which forms the western boundary of the Northern Planning Area, will be the primary access corridor to and from the planning area. This corridor is also the major north-south arterial in the City, providing the only direct, through connection to the City of Healdsburg from Area A. A gateway entry road will provide the primary, direct connection from Area A to Healdsburg Avenue. Internal streets will provide direct connections to the entry road and indirect connections to Healdsburg Avenue from the residential neighborhoods in Area A. Connections with the existing streets in neighborhoods to the south and east of Area A will be made to improve emergency access

and reduce overall traffic volumes on existing streets. However, connections will be designed to discourage through traffic.

In March 1990, the City commissioned the *North Healdsburg Area Traffic Study* to identify the necessary improvements to accommodate access and circulation to North Healdsburg (including the Northern Planning Area) via Highway 101 and City arterials. Among the improvements recommended was the construction of the Grove Street extension to connect to Healdsburg Avenue at Chiquita Road. The Study also recommended modification of the existing Dry Creek Road interchange and adjacent arterial streets serving the interchange. Upgrading the Dry Creek Interchange will require additional improvements to Dry Creek Road between Highway 101 and Healdsburg Avenue, and to Healdsburg Avenue north and south from Dry Creek Road in order to serve the increased demand for freeway access from both the north and south. Dry Creek Road will ultimately be improved to five lanes and traffic signals will be required at the ramp junctions of the US 101/Dry Creek interchange.

Healdsburg Avenue must be widened to 5 lanes from Dry Creek Road north to the intersection with the Area A entry Road. Along this stretch, the intersections of Healdsburg Avenue with the Area A entry road and Healdsburg Avenue and Chiquita Road will both require signalization. With these improvements a level of service "C" or better is anticipated. North of the Area A entry road intersection, Healdsburg Avenue will be widened to three lanes.

Implementation of the improvements to Healdsburg Avenue and the Dry Creek Interchange are tied to the development of the Northern Planning Area. Improvements to the Dry Creek Interchange and arterial roads must begin before 600 units are completed in the Northern Planning Area.

### **Objectives**

- Secure as needed, the following street connections to accommodate the logical phasing and development of northern Healdsburg:
  - Through easement
    - to the Roberts property through the Moreno and Stritzel properties
  - Through rights-of-way dedication
    - from Healdsburg Avenue to Area B through the Z.A.M. properties to the northeast corner of Area A.
    - to Area B through the Z.A.M., Banducci, Brush and Giovannoni properties
    - to Rosewood through, Banducci, Englert, McNulty, Engelhardt, Garaventa, Unciano and Vinecrest Partnership properties.
    - to Poppyhill through the Unciano property
    - to Healdsburg Avenue on the Vinecrest Partnership property adjacent to the Gomez, Moreno, Stritzel, Thompson and Garaventa properties

### **9.1.2 GRID PATTERN OF STREETS IN THE NEIGHBORHOOD CENTER**

#### **Policy**

- In the more level northern portion of Area A, identified as the Neighborhood Center, create a grid pattern of streets that will define a well-structured, interconnected neighborhood modeled after the street pattern in historic Healdsburg.
- Where topography permits, continue the grid pattern of streets into adjoining areas (i.e., Area C) to create a continuity in the pattern of development in the Northern Planning Area.

Historic Healdsburg, like many western American towns, was platted in a grid. The grid allowed the town to grow and in an orderly yet flexible way by providing both a recognizable and highly interconnected network of streets.

The grid continues to provide important opportunities to create a highly interconnected neighborhood with a recognizable and cohesive structure, while also referencing the traditional town form. The grid facilitates the movement of traffic by providing many ways to move from one place to another, contributing to traffic efficiency by dispersing traffic over many streets rather than concentrating it onto a few.

### **9.1.3 HEALDSBURG AVENUE**

#### **Policy**

- Recognizing the need to widen Healdsburg Avenue in the future, seek to maintain the scenic and rural character of Healdsburg Avenue adjacent to the Northern Planning Area by minimizing its width and enhancing frontage with landscaping.

#### **Objective**

- Where widening requires retaining walls, face these walls with native stone and/or provide landscape planters designed as part of the walls to minimize the visual intrusion of the walls in the scenic corridor.
- Widening Healdsburg Avenue in accordance with accepted design standards to maintain a level of service "C".

Any disturbance of vegetation must be replaced with the native vegetation. Retaining walls required to maintain or stabilize cuts or slopes shall be faced with native stone and/or landscaped planters to conceal the visual presence of these walls. These design treatments shall be required to maintain the scenic and gateway function of the road as designated by the City. The replanting and stone walls undertaken along the Simi Winery frontage opposite the planning area provides an excellent example of a sensitive roadside treatment in character with the rural/scenic qualities associated with this northern gateway to the City of Healdsburg.

#### **Standards**

- a. Healdsburg Avenue should be widened the minimum number of lanes required to meet the City requirements of level of service "C". The design of the roadway widening should reflect the status of the street as a scenic corridor.
- b. Where practical, minimize major grading that destroys significant stands of vegetation or natural rock outcroppings.
- c. Provide a landscaped buffer with a minimum width of 30 feet adjoining Healdsburg Avenue new edge of pavement. This buffer shall be landscaped with appropriate native and compatible non-native trees, shrubs and groundcovers, e.g. redwoods, oaks, ferns, in character with both the existing landscape (especially that along the Simi Winery frontage) and the "scenic" designation of Healdsburg Avenue while preserving desirable native trees and vegetation. The landscaped buffer shall be designed to screen views of nearby houses from the road, while allowing interspersed views of hillsides and ridges. Maintenance of this required landscaped buffer shall be the responsibility of the property owners, either through a neighborhood association or through the formation of a landscaped and lighting assessment district.
- d. Require native stone facing, landscape planters or other design treatments as part of any retaining walls required to stabilize slopes or cuts along the right-of-way.
- e. Minimize the construction of sound walls along Healdsburg Avenue. Maintain a minimum 65-foot setback from Healdsburg Avenue (edge of pavement) to mitigate sound levels where berms or non-habitable structures are used to buffer development, or where acoustical construction materials and design features are used to maintain interior noise levels of 45  $L_{dn}$  or less. Where no berms or non-habitable structures are employed, a minimum

170-foot setback would be required to mitigate noise levels for habitable structures. For all developments within the 170-foot setback, a detailed acoustical analysis shall be required to document and describe how interior noise levels would be achieved.

- f. Prohibit parking on Healdsburg Avenue in the area adjacent to Area A to avoid conflict with high traffic volumes and the "scenic" designation of the road and to minimize the width to that required for travel lanes.

#### **9.1.4 CURBCUTS**

##### **Policy**

- Ensure public safety through good roadway design by eliminating dangerous curbcuts for private driveways.

##### **Objective**

- Eliminate dangerous curbcuts to enhance the safety of residents and visitors to Area A by reducing conflicts with through traffic on Healdsburg Avenue.

##### **Standards**

- a. Replace existing driveway curb-cuts on Healdsburg Avenue with driveway access for existing and future parcels. New Driveway access shall be via new streets internal to Area A, where practical, to increase public safety and welfare and as indicated by the Plan. Existing driveway curb-cuts on Healdsburg Avenue create dangerous traffic movements due to poor visibility and lack of sufficient shoulder and turning lanes. Subdivision of properties that have curb-cuts on Healdsburg Avenue shall require that access to existing dwellings and new lots created be taken from the new north/south internal public street network in Area A.
- b. Provide 2 new, signalized intersections on Healdsburg Avenue to accommodate growth in Area A and the rest of the Northern Planning Area: one at the main entry road to Area A, and one at Chiquita Road.
- c. Limit street access to Area A from Healdsburg Avenue to the main entry road and the smaller local-serving street at the southern end of the planning area.
- d. Direct access from private driveways to Area A's east-west collector (i.e., the entry road) is prohibited.
- e. Provide adequate intersection configurations, sight distances and adequate turning radii, but avoid overdesigning improvements that destroy rural character.
- f. Separate pedestrian and bicycle paths from auto traffic where possible.
- g. Ensure adequate emergency vehicle access to development areas and potential wildland fire areas.
- h. Limit street grades to a maximum slope of 15 percent. In difficult terrain, grades up to 20 percent will be acceptable for short distances not to exceed 300 feet, and shall be designed appropriately for fire fighting equipment to the satisfaction of the fire department. Cul-de-sac lengths should not exceed 400 feet in length without secondary access.

#### **9.1.5 PARKING**

##### **Policy**

- Provide adequate parking for residents and guests, while allowing sufficient flexibility to meet urban design objectives.

##### **Objective**

- Apply existing City parking requirements to ensure sufficient parking in Area A. Adjustments to existing standards may be acceptable when needed to meet Specific Plan urban design or housing element objectives.

### **Standard**

- a. Meet the minimum standards for on-site parking spaces per unit as established in Article 12, Section 1200 of the municipal code.
- b. Existing minimum off-street parking space requirements are as follows:
  - single-family dwellings and dwellings in condominium ownership: 2 spaces/unit
  - all others: 1.5 spaces/unit
- c. Where appropriate, provide on-street parking to meet additional parking demand and to slow traffic. The location and dimensions of on-street parking appear in the *Design Standards* in this section. On-street parking shall be located to preserve sensitive environmental features such as trees, steep slopes, etc.
- d. Encourage the sharing of parking, especially between the Park and School.

## **9.2 Public Transit**

### **Policy**

- Ensure the provision of convenient transit services and facilities for Area A residents.

### **Objective**

- Reduce the reliance on the automobile by providing other forms of transportation for residents including youth, senior citizens and those with lower incomes.

The Plan encourages the integration of Area A into County and City transit systems which currently run buses past the planning area along Healdsburg Avenue. In the long-term, rail transit along the Northwestern Pacific Railroad, west of the Northern Planning Area may also provide a possible transit option.

### **Standard**

- a. Secure pleasant and convenient transit stops in strategic locations within Area A, particularly in the higher density areas.
- b. Encourage continued public transit service through north Healdsburg.
- c. Encourage the extension of rail service along the Northwestern Pacific Railroad rights-of-way and the creation of a station within walking distance of Area A.

## **9.3 PEDESTRIAN AND BICYCLE CIRCULATION**

### **Policy**

- Establish a safe, convenient and pleasant network of paths, trails and lanes for pedestrians and bicyclists.

### **Objective**

- Construct a network of pedestrian and bicycle paths, trails and lanes to reduce reliance on the automobile and enhance the use and appreciation of Area A's unique environment.

The Northern Planning Area's natural features and open space offer a special opportunity to create attractive, high-quality paths and trails.

### **Standard**

- a. Connect major activity areas involving recreation, education, shopping, and housing with pedestrian paths and bicycle trails.
- b. Where possible, align paths and trails to take advantage of natural features and open space.
- c. A pedestrian sidewalk and/or a Class I bicycle path may be set back from Healdsburg Avenue within the required 30' landscaped buffer, unless major grading is needed. .
- d. Where possible, separate pedestrians and bicyclists from vehicular circulation.  
(Figure 9A)

## **9.4 AESTHETIC AND FUNCTIONAL CONSIDERATIONS**

### **Policy**

- Recognize the importance of streets and roads as an integral part of the City's image and identity, and design streets to establish attractive and identifiable pedestrian and vehicular linkages.
- Establish a hierarchy of streets and trails with consideration given to both functional and aesthetic characteristics.

### **Objective**

Use streets and trails to link Area A to downtown Healdsburg, and establish a distinct character of this new neighborhood.

### **Standards**

- a. Create a hierarchy of streets based on functional and aesthetic considerations. The hierarchy should clearly reflect the different functions of the streets comprising the network, and should address vehicular, bicycle and pedestrian traffic, existing terrain, amenities, adjoining uses and components of the City's open space system.
- b. Create a hierarchy of streets that acknowledges and enhances the pedestrian environment and does not accommodate vehicular traffic to the detriment of pedestrian convenience or safety.
- c. Establish design standards for streets within the hierarchy that address right-of-way and pavement width, parking, landscaping and sidewalks, bicycles, parks and open space.
- d. Limit the use of curbs in environmentally sensitive areas, where practical, to preserve these areas from excessive grading or digging and to reduce the visual impact of roads.

## **9.5 DESIGN STANDARDS**

### **9.5.1 HIERARCHY OF STREETS**

Streets strongly influence the character and identity of an area. While it is essential that streets safely accommodate vehicles, they must also accommodate residents as public spaces along which people move between home, work, play and shopping. Street design therefore needs to address not only the street's function, but also its qualities as a public space experienced by pedestrians, bicyclists and drivers. These qualities deserve strong consideration in the development of design standards, especially since the City can ensure the quality of streets through direct municipal control.

Streets in Area A are classified according to an hierarchy. This hierarchy assigns streets into categories based on functional requirements and their surrounding context (both natural and developed). Standards have been developed which address aesthetic and functional requirements simultaneously and place appropriate emphasis on the vehicle,

the pedestrian and on the Area's image and identity.

Because Area A contains distinct development areas characterized by different topographic conditions, street standards are organized by development area. The standards address anticipated use, as well as respond to the character of each development area.

The standards describe the dimensions and the arrangement of key features including travel lanes, parking, pedestrian paths and landscaping. Special conditions are also addressed, such as steep slopes and creekside open space.

Deviations from these standards and dimensions shall be allowed upon a determination by the Public Works Director that safe and adequate public access and circulation are preserved by such deviations. Such deviations may be proposed by the City or a private landowner based on functional, aesthetic and environmental considerations, such as the need for limiting unnecessary pavement, accommodating bikelanes, or preserving environmentally sensitive areas or resources.

Concrete curbs and gutters will be provided on all streets except where excessive disturbance of existing vegetation, aesthetics or other very low traffic levels are a consideration. In this case, an acceptable methodology for stabilizing the edge of the street or road shall be determined with the City.

The following categories make up Area A's hierarchy of streets. The names of the categories are intended to reflect the character of the roadway. Additional standards apply to pedestrian paths not provided within street standard.

*(Figure 9B)*

### **Healdsburg Avenue**

Healdsburg Avenue is the City's main north-south arterial, serving high traffic volumes (over 10,000 Average Daily Trips or ADT) at high speeds. Adjacent to the planning area, the roadway currently has two travel lanes within a 60-72 foot right-of-way. With buildout of the Northern Planning Area, the roadway would need to be widened:

- between the intersection of Dry Creek Road north and Area A Entry Road to 5 lanes with 2 through lanes in each direction and a continuous center turning lane to accommodate left turns (84 foot right-of-way).

*(Figure 9C)*

- between the Entry Road and the Area A property line to 3 lanes with 1 lane in each direction and a continuous center turning lane to accommodate left turns (62 foot right-of-way).

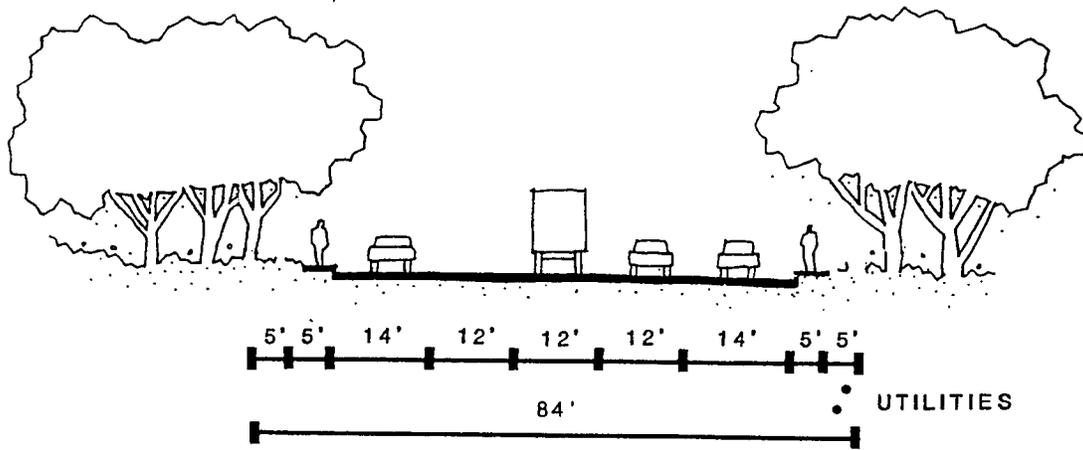
*(Figure 9D)*

Healdsburg Avenue has been designated as a scenic corridor for its rural character along Area A's western boundary. Parking shall be prohibited along the Healdsburg Avenue frontage. Wherever possible, sound walls should be avoided in favor of landscaped berms and plantings of vegetation to buffer noise. Landscaping will be provided within the right-of-way wherever possible, in addition to landscaping required on adjoining properties to together provide a minimum 30-foot-wide landscaped buffer on the east side of the roadway. Sidewalks will be required on both sides of the road. Steep rock outcroppings along the northern property line at Healdsburg Avenue should be disturbed as little as possible to preserve the scenic qualities of the road.

### **Entry Road**

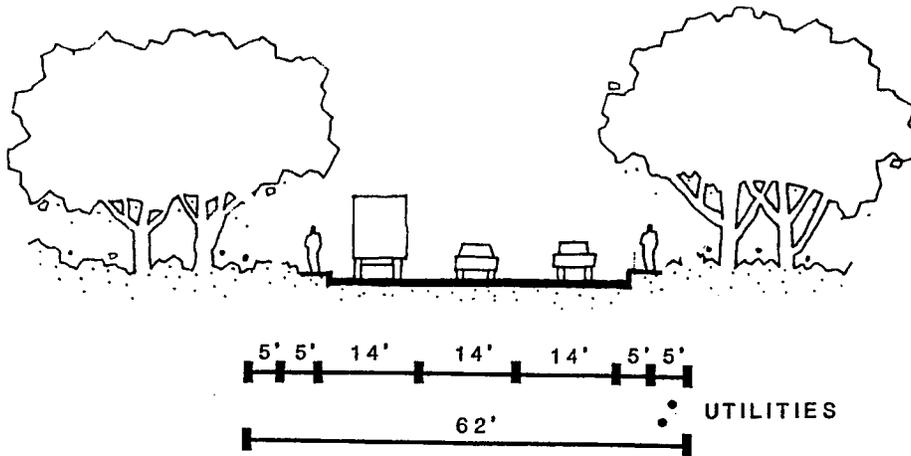
The Entry Road to Area A will extend from Healdsburg Avenue to the easterly boundary of Area A. Upon development of Area B, the Entry Road will be extended east to connect to the Area B roadway system. The Entry Road will serve traffic volumes of over 2000 ADT at moderate speeds.

At the intersection with Healdsburg Avenue, a four-lane road with a central planted median shall be provided within a 100-foot right-of-way to allow for turning movements from and to Healdsburg Avenue. The 100-foot right-of-way shall extend to the first intersection in the Neighborhood Center subarea. After this initial section, the Entry Road will be a 3-lane road, one travel lane in each direction with a continuous, center turning lane. Curb to curb the road will be 58 feet wide where it is a four lane facility, centered with an overall right-of-way of 100 feet, and 42 feet wide where it is a three lane facility, centered within an overall right-of-way of 84 feet. A 5-foot sidewalk will be provided on each side of the Road with the remainder of the right-of-way for landscaping. The sidewalk does not have to run parallel to the curb but may meander gently within the overall area provided for the sidewalk and planting area. The intent is to preserve the general "country" feeling in Area A, as well as to provide flexibility in



HEALDSBURG AVENUE

Figure 9C



HEALDSBURG AVENUE

Figure 9D

**STREET DIMENSIONS**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
 Area A

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preserving existing vegetation along the right-of-way especially through the Ridgeland area.

*(Figure 9E)*

At the intersection with Healdsburg Avenue, where the Road widens to four lanes, a planted median strip and landscape strips on either side of the right-of-way will be required. These plantings should be designed with a grid pattern of trees or shrubs to emulate the orchards and vineyards found in the surrounding area.

*(Figure 9F)*

Direct driveway access to the Entry Road and parking along the Road are prohibited. A bicycle lane may be provided as part of a striped lane within the cartway.

### **Country Road - Two-Way Traffic**

Eight streets within Area A, primarily providing access to the Ridgелands, have been designated Country Roads. This category is meant to serve traffic volumes under 2000 ADT at moderate-to-slow speeds, yet still maintain a distinct rural character, like most rural roads in Sonoma County. The designated country roads pass through hilly terrain or through open space whose rural character is to be maintained.

Two roads extending south from the Entry Road will be designated Country Roads with a 62-foot right-of-way for two 14 foot travel lanes. The wide right-of-way is maintained to assure that buildings are located well back from the street and that there is design flexibility within the right-of-way for street design that maximizes the protection of existing vegetation.

Parking and soundwalls along all Country Roads shall be prohibited. A sidewalk meandering along only one side of the roadway should be provided to maintain a rural character for the road. A bicycle lane may be provided as part of a striped lane within the cartway or as a part of pedestrian path adjacent to the cartway. Formal street planting is prohibited and native plant material should be used to revegetate areas within the right-of-way disturbed by construction.

Sidewalks will be required on one side of the street due to the limited amount of traffic and the presence of many sensitive natural resources in this area which should not be disturbed.

*(Figure 9G)*

### **Country Road - One Way**

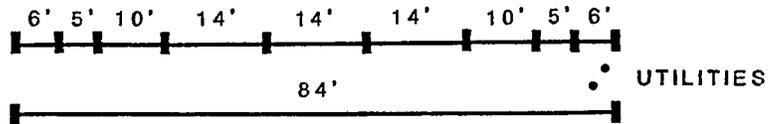
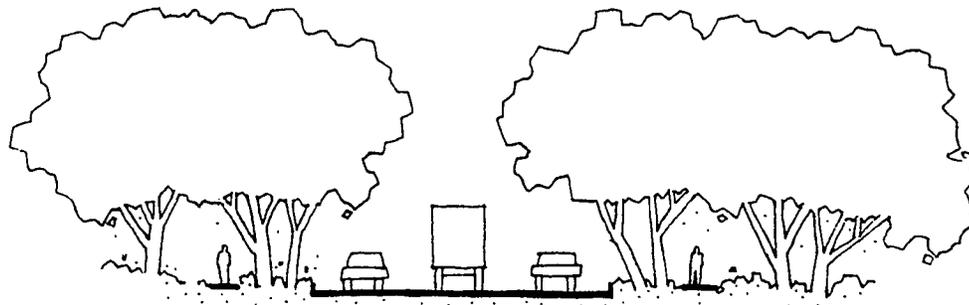
Although none has been proposed as yet, the one-way Ridgелands residential street designation was developed to serve those areas with severe environmental constraint, that would suffer impacts from the construction of a two-lane roadway. The one-way Ridgелands street would provide a single 20-foot travel lane within a 32-foot right-of-way. Variances to this minimum pavement width intended to preserve environmentally sensitive features and/or reduce grading impacts may be allowable if approved by the City Fire Department. The roadway would maintain a rural character with no on-street parking and no through-traffic.

*(Figure 9H)*

### **Primary Residential Street**

This designation is intended to provide a street with a neighborhood character (i.e. more urban in character), with on-street parking and through-traffic (over 2000 ADT) permitted. These streets, which are located in the higher density areas of Area A, include the northern extension of Rosewood Drive in the Neighborhood Extension subarea, and the north/south street bordering the east side of the proposed park and school sites connecting the entry roads of Areas A and C in the Neighborhood Center subarea. These streets provide two 12 foot travel lanes with an 8 foot parking lane on either side. A 5-foot sidewalk with a 6-foot planting strip would also be provided. The overall street right-of-way would be 62 feet wide. Landscaping along these streets should include a regularly spaced or formal planting of trees in the planting strip.

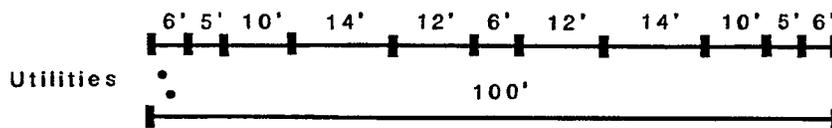
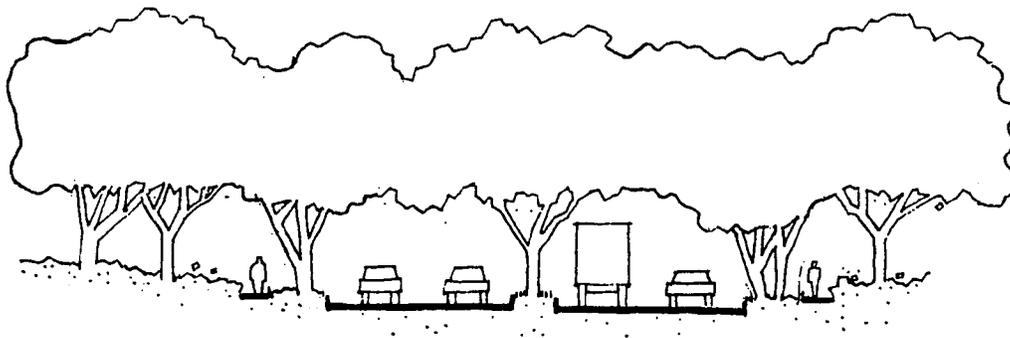
*(Figure 9I)*



**ENTRY ROAD**

**3 Lanes with Center Turning Lane**

Figure 9E



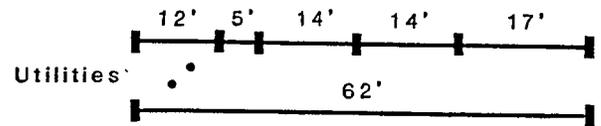
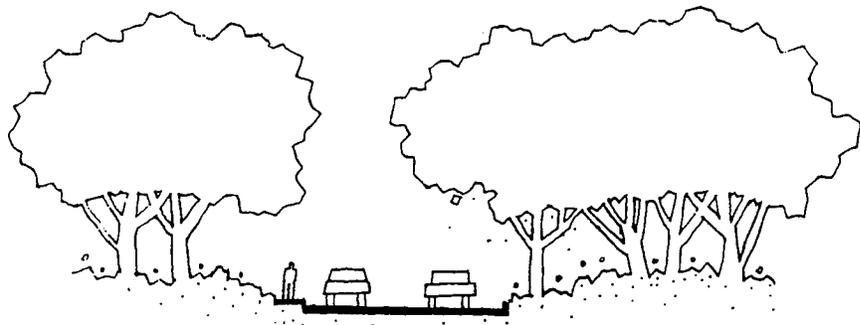
**ENTRY ROAD**

**4 Lanes at Intersection with Healdsburg Avenue**

Figure 9F

**STREET DIMENSIONS**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
 Area A

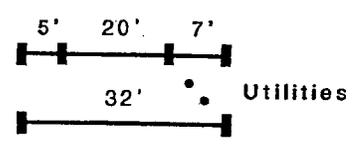
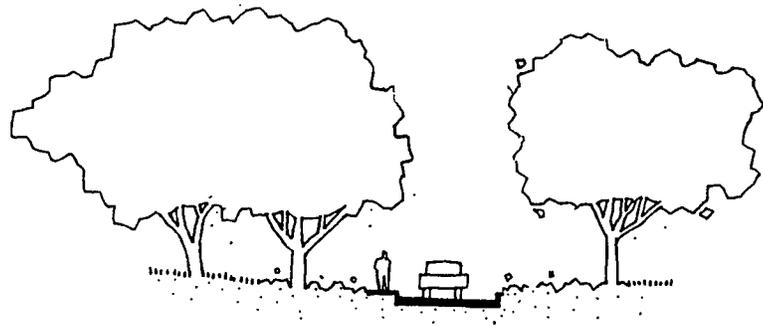
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**COUNTRY ROAD**  
Two Way

Note:  
Sidewalk can meander  
away from street within  
right-of-way.

Figure 9G



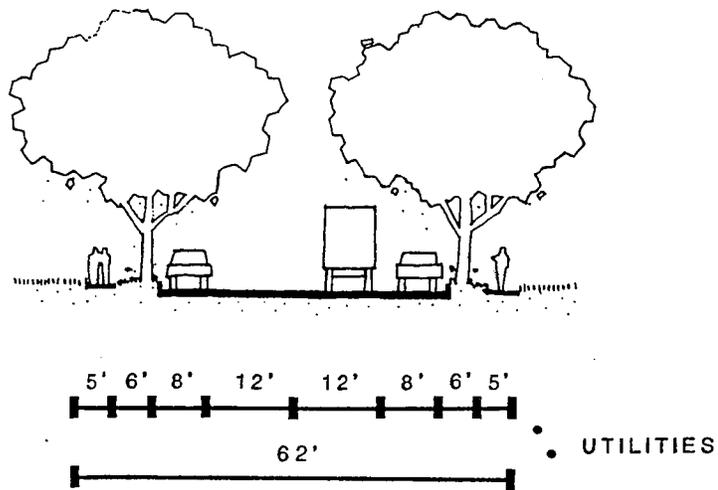
**COUNTRY ROAD**  
One Way

Note:  
Sidewalk can meander  
away from street within  
right-of-way.

Figure 9H

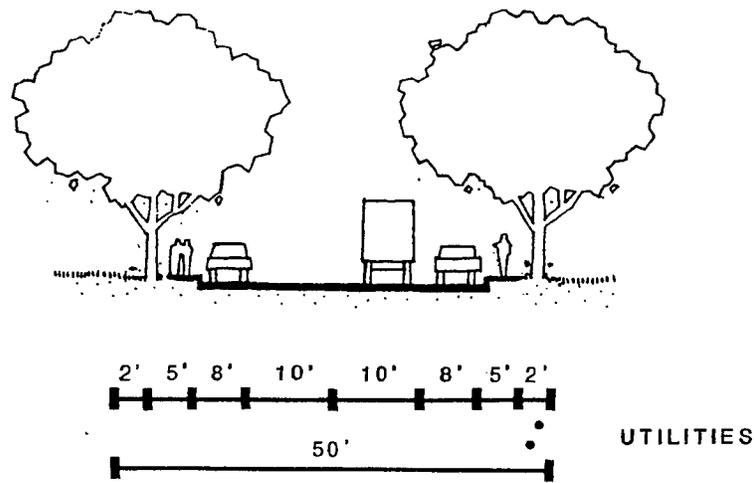
**STREET DIMENSIONS**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

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PRIMARY  
RESIDENTIAL STREET

Figure 9I



SECONDARY  
RESIDENTIAL STREET

Figure 9J

**STREET DIMENSIONS**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

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### **Secondary Residential Street**

This designation is also intended to provide a street with a neighborhood character (i.e. more urban in character), with on-street parking and through-traffic (over 2000 ADT) permitted. These streets, also located in the higher density areas of Area A, provide two 10 foot travel lanes with an 8 foot parking lane and a 5 foot sidewalk on either side. The overall street right-of-way would be 50 feet wide. Landscaping along these streets should also include a regularly spaced or formal planting of trees within or adjacent to the sidewalk.

*(Figure 9)*

### **Private Driveways**

This designation identifies those existing private drives which will remain and be incorporated in the plan. All of the designated driveways are located in the hilly southern half of the planning area. These drives have been favored in situations where development of a public street is considered inappropriate given the limited number of units served. The driveways would not accommodate any through traffic.

**HIERARCHY OF STREETS**  
**Design Standards**

<b>Type of Street</b>	<b>R.O.W.</b>	<b>Travel Lanes</b>	<b>Parking</b>	<b>Sidewalk Planting Strip or Median</b>	<b>Figure #</b>
<b>Healdsburg Avenue</b>	84'	2@14' 2@12' 1@12' for left turn lane	None (1)	2@5' sidewalk 2@4.5' planting strip	<b>9C</b>
	62'	2@14' 1@14' for left turn lane	None (1)	2@5' sidewalk 2@4.5' planting strip	<b>9D</b>
<b>Entry Road</b>	84'	3@14'	None (1)	2@10' planting strip 2@6' sidewalk 2@5' planting strip	<b>9E</b>
	100'	2@14' 2@12'	None (1)	2@5' sidewalk 2@5' planting strip 1@6' planted median or turning lane as required	<b>9F</b>
<b>Country Road</b>	62'	2@14'	None (2)	1@5' sidewalk 1@12' planting strip 1@17' planting strip	<b>9G</b>
<b>Primary Residential</b>	62'	2@12'	2@8'	2@5' sidewalk 2@6' planting strip	<b>9I</b>
<b>Secondary Residential</b>	50'	2@10'	2@8'	2@5' sidewalk	<b>9J</b>

(1) No parking permitted due to traffic volume.

(2) No parking required as streets serve less than 10 dwellings and lots are large enough to accommodate guest parking.

## **9.5.2 WIDTH OF PAVEMENT**

### **Policy**

- To maintain the rural character of Area A, keep the width of street pavement to a minimum, consistent with safety standards and level of service requirements.

### **Objectives**

- Determine pavement width based on the actual level of traffic to be served, and the street's designation within the street hierarchy.
- Provide adequate right-of-way to permit flexibility in responding to future needs for street widening and to accommodate any minimum level of service, speed, and safety requirements.

In general, minimizing street pavement achieves the following objectives:

- reduces impacts to environmentally sensitive areas;
- discourages high speeds;
- facilitates visual enclosure of the street by tree canopies;
- visually emulates small town and rural environments;
- provides more space for landscaping.

Parking requirements can be reduced in ridgeland areas where larger lots and lower densities reduce on-street parking demand and additional on-site parking can be accommodated. Parking bays at irregular intervals on-street may also be provided.

Landscaped rights-of-way are required, especially where extensive grading or disturbance through street construction occurs. Primary residential streets in intensively developed areas such as the Neighborhood Center and Neighborhood Extension areas require planting strips-landscaped separations between the curb and sidewalk in which street trees and groundcover are planted.

In the Ridgелands, the character of wooded areas should be preserved. The abundance of naturally occurring vegetation contrasted with formalized street-side planting areas would be inappropriate. Vegetation disturbed by roadway construction should be replaced to return the right-of-way to its natural appearance before construction.

## **9.5.3 SPECIAL DESIGN REQUIREMENTS**

### **Policy**

- Within the street hierarchy, designate the Entry Road to receive enhanced design treatments in response to its unique function as a gateway entrance into Area A.

### **Objectives**

- Establish special design requirements to ensure that the Entry Road contributes to the image and identity of Area A.

The Entry Road shall be designed to have the visual appearance of a rural road with natural landscaping and no parking. It should provide an entry corridor that enhances and maintains the rural setting of the area. Pedestrian sidewalks shall be provided on both sides of the right-of-way.

### **Standards**

- b. Preserve and enhance native landscaping within this right-of-way, except along the northern edge of the road within the Neighborhood Center and within the Gateway, where a cultivated rural landscape treatment such as

orchards, vineyards, or wild flowers is encouraged. The use of rustic fencing treatments is encouraged. (Figure 9K and 9L)

## **9.5.4 SIDEWALKS AND TRAILS**

### **Policy**

- Provide sidewalks and trails to accommodate pedestrian circulation and enhance the character and livability of Area A.  
The alignment for sidewalks or trails shall be responsive to natural constraints.

### **Objectives**

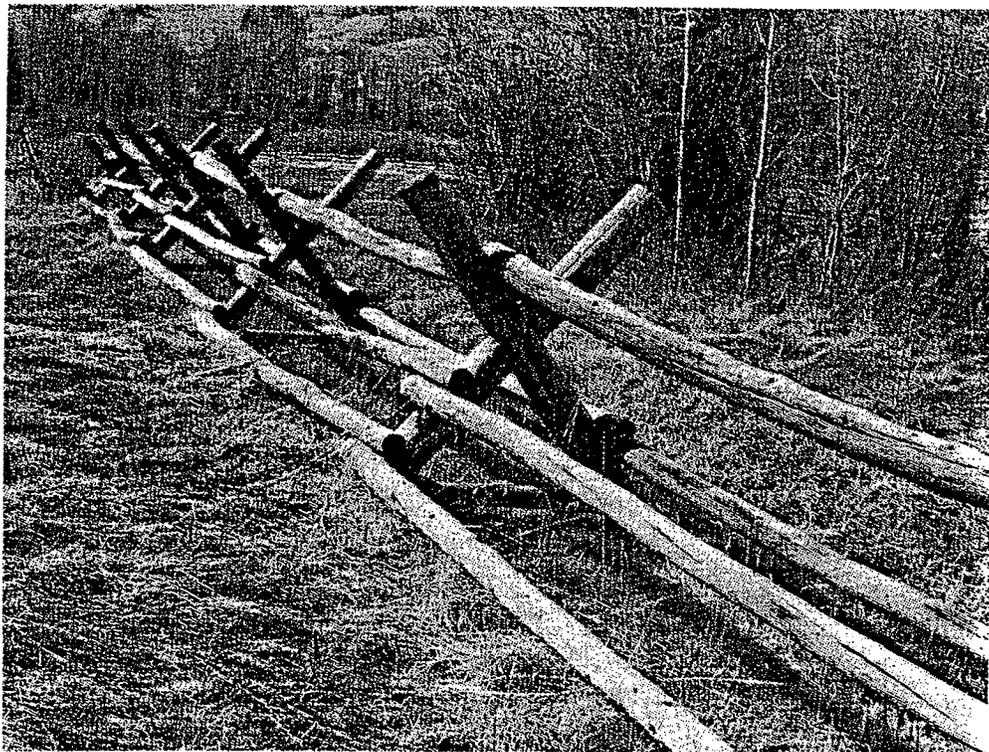
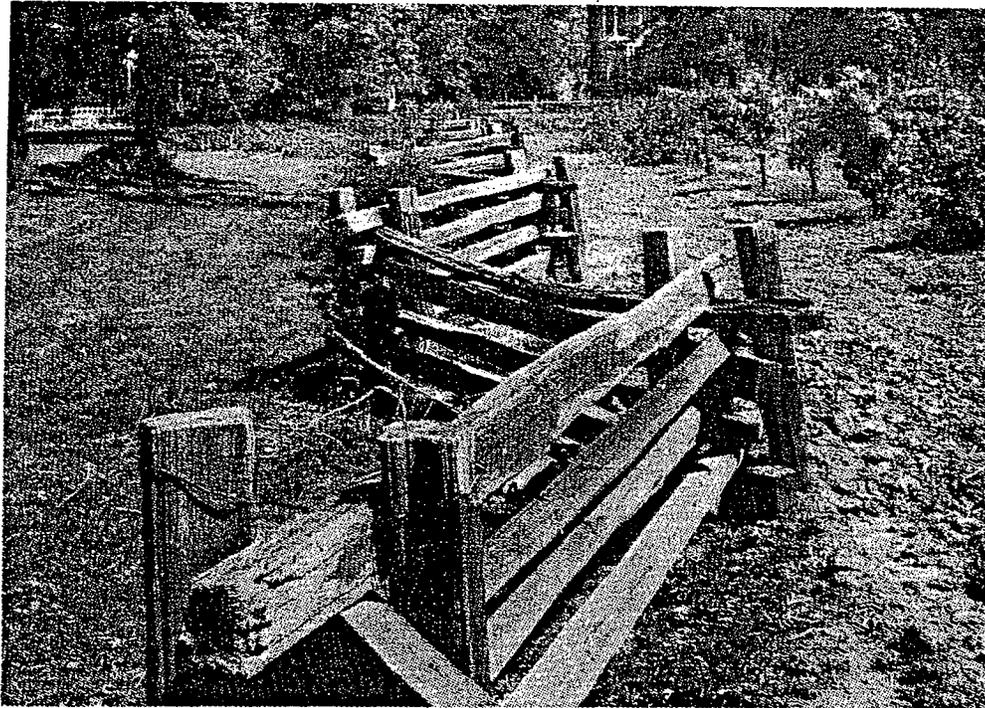
- Develop sidewalks and trails that provide for efficient pedestrian and bicycle circulation, with a design that is compatible with the character of their surroundings.

In the Neighborhood Center, straight narrow sidewalks will be required to relate to the formal siting of building facades and entries on the street. In more rural areas and where there is steep topography, locating sidewalks on only one side of the street is preferred to minimize grading and destruction of vegetation. All sidewalks and trails shall be paved. Sidewalks shall be concrete, brick or similar material and shall conform to City standards. Paths and trails may use asphaltic concrete paving.

In areas of flat or relatively flat topography, design sidewalks and trails to parallel streets and to avoid unnecessary meandering, undulating, or creation of berms. In areas of steep topography, sidewalks and trails shall be designed to minimize unnecessary grading or destruction of vegetation. Sidewalk and trail alignments should be designed to avoid such constraints. Revegetation with native and/or appropriate plant materials of disturbed areas is required.

### **Standards**

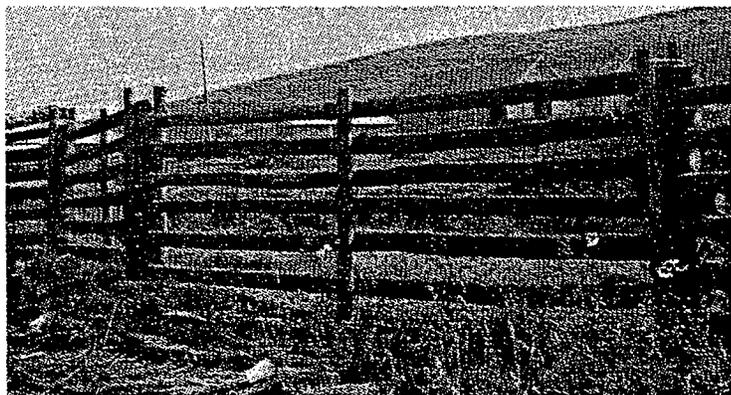
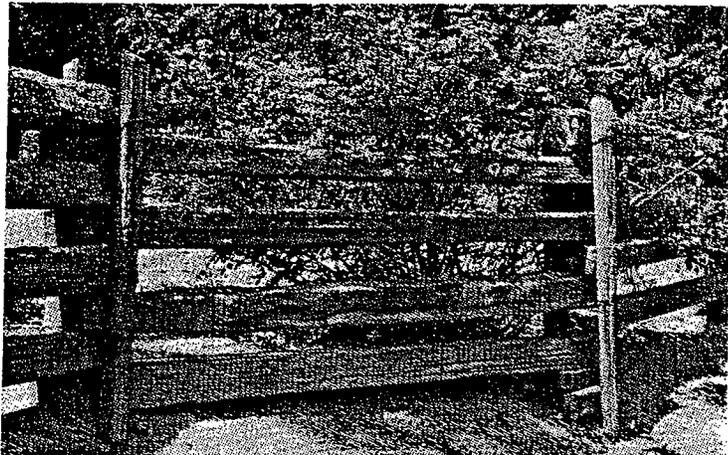
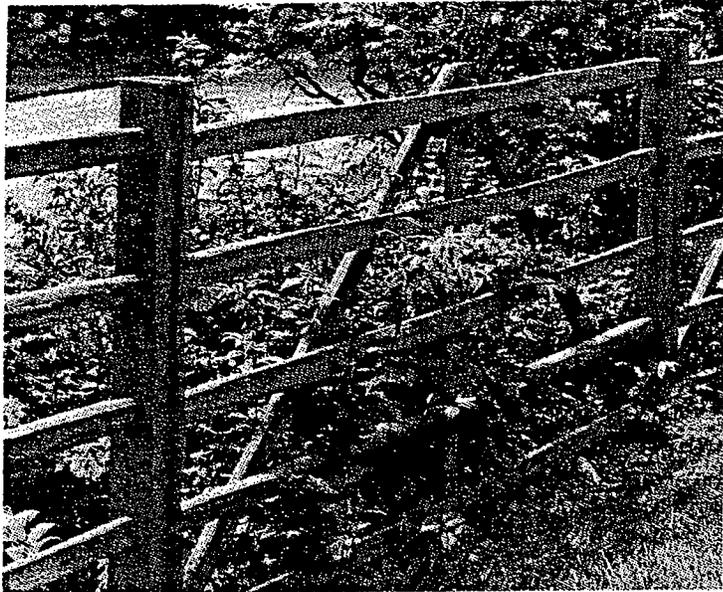
- a. Sidewalks
  - i.) Sidewalks shall be required along streets as depicted in typical street cross-sections (see figures for each street designation).
  - ii.) In the Ridgeland, sidewalks which meander along streets are preferred in areas with difficult terrain or sensitive vegetation. Sidewalks shall be 4 feet in width and provided on only one side of streets due to severe topographic constraints and environmentally sensitive vegetation and wildlife habitat.
- b. Trails
  - i.) Trails shall provide access to public open space and connect areas without sidewalks.
  - ii.) The relationship of trails to roads and creeks may vary depending on conditions, especially topography. Trails shall have a cleared right-of-way of 10 feet (if appropriate for existing conditions) to allow for passing pedestrians and bicyclists. A minimum of 4 feet of the right-of-way shall be paved. Where trails parallel creeks, the 10-foot trail right-of-way shall be added to the 25 foot setback from the creek for a total setback of 35 feet.
  - iii.) Trails alignments shall be located to protect the privacy of adjacent properties. Fences can be placed on property lines between public trails and residences, where separation is not provided by natural features such as creeks, steep slopes or heavy vegetation.
  - iv.) Trails are intended for use by both bicyclists and pedestrians and shall have clearly marked "Give Way" signs, informing bicyclists that they must yield for pedestrians. Trails shall have a minimum paved width of 4 feet.



**FENCING**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

Wallace Roberts & Todd  
Jones & Stokes Associates

Figure 9K



**FENCING**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

Wallace Roberts & Todd  
Jones & Stokes Associates

Figure 9L

### **9.5.5 STREET TREES**

#### **Policy**

- Provide street trees and street landscaping on all streets consistent with the character of the area within which the street is located.

#### **Objective**

- Select street trees to provide shade as well as a pleasant natural enclosure of the street. Regular spacing of canopy trees often characterizes traditional neighborhoods. In rural areas, irregular or natural clusters of native trees are more typical.

#### **Standards**

- a. In the Neighborhood Center and Neighborhood Extension, plant street trees within the required planting strip, or along the edge of the street right-of-way where no planting strip is required, at a minimum distance of 20 feet on center but no more than 40 feet on center. Trees should be planted in the center of the planting strip where such a strip is required. Deciduous trees are encouraged to highlight changing seasons, and to provide summer shading and an open canopy for winter sun and warmth.
- b. In Ridgeland and along the Entry Road and Healdsburg Avenue, require streetside planting that is rural in character, such as groups of native trees like oaks or redwoods in a natural-appearing manner (e.g., in groups or clusters). Such plantings would occur in private property adjacent to the street right-of-way or within the right-of-way if there is no conflict with underground utilities. Within the right-of-way itself, native shrubs, grasses and wild flowers shall be planted to revegetate land disturbed by construction.

### **9.5.6 LIGHTING**

#### **Policy**

- Minimize street lighting within specific set safety standards in Area A to maintain the rural character of the area.

#### **Objective**

- Maintain a small town and rural character in Area A while meeting the needs for public safety.

Within the planning area, street lights should be used to visually punctuate and enhance public safety at intersections. Beyond intersections, a low level of illumination is desired and is an important rural characteristic.

#### **Standards**

- a. In order to ensure public safety and welfare, on Healdsburg Avenue and other major thoroughfares, street lighting systems should be designed and engineered to appropriate set safety standards.
- b. In the Ridgeland and Gateway areas, limit street lighting to minimum needed to meet specific set safety standards.
- c. In the Neighborhood Center and Neighborhood Extension, space street lights more closely and regularly as part of the overall street design. Provide decorative fixtures appropriate to the character of the Neighborhood Center.
- d. Avoid lighting trails through open space to discourage nighttime use and to maintain the area's rural character.
- e. To better contain illuminated areas and reinforce the area's small town and rural character, limit light poles to 16 feet in height.
- f. In conjunction with City Underground District No. 2, a specified portion of the street lights on Healdsburg Avenue

will be at the expenses of the Area A developers. All street lights within the balance of Area A will be at the expense of the Area A developers.

### **9.5.7 UTILITIES**

#### ***Policy***

- Overhead utilities degrade the quality of environments by increasing visual clutter, reducing landscaping and interfering with views.

The existing 60 kv line will remain above ground but be relocated to run with proposed roadway alignments.

#### ***Objective***

- Place all utilities underground where feasible and practical.
- Place any above-ground utilities within planter strips or behind sidewalks to prevent obstruction of sidewalks. • Maintain adequate distance between the 60 kv line and adjacent residences either through appropriate setbacks, height of power lines, and/or any other mitigating line construction techniques to minimize any potential health risk of electro-magnetic fields (EMF) to residents. The minimum effort on part of the utility shall be consistent with the most recent order, in regards to public exposure to EMF, by the California Public Utilities Commission (CPUC).

# **10. PUBLIC FACILITIES ELEMENT**

# 10 PUBLIC FACILITIES ELEMENT

The Public Facilities Element anticipates public facilities in Area A and sets forth related Policies and Objectives. The Public Facilities Element is organized in sections relating to stormwater detention, utilities, fire, police, and schools.

The goals in the Public Facilities and Services section of the General Plan (Part II, Section V, pp. 43-48) pertaining to the adequate provision of water, stormwater, sewage, electrical, police, fire and educational facilities are applicable to the Specific Plan Area.

## 10.1 DRAINAGE FACILITIES

The Plan requires the installation of all basic storm drainage facilities, including pipes, culverts, inlets, manholes, open channels, and swales.

(Figure 10A)

### 10.1.1 NORTH DETENTION BASIN (FOSS CREEK DETENTION FACILITY)

#### Policy

- Provide area-wide flood protection facilities to mitigate impacts associated with increased runoff resulting from development of the Northern Planning Area.

#### Objective

- Construct a North Detention Basin in the first phase of construction of Area A.

All of Area A, as well as most of Areas B and C, lie within the Foss Creek drainage basin. Foss Creek flows directly through central Healdsburg. Construction of a North Detention Basin at a location adjacent to Foss Creek just up stream of Healdsburg Avenue has been planned as part of the City of Healdsburg's flood control program for Foss Creek since at least 1983. The construction of a detention basin at this location is described in a 1983 report entitled "Foss Creek Detention Basin, Preliminary Design Study", prepared by Phillip Williams & Associates. The plan is also a part of the 1987 General Plan.

The 1983 Williams report describes an integrated flood control plan for the Foss Creek watershed, relying upon the combined performance of two detention basins - the City's existing Grove Street Detention Basin and the North Detention Basin. In order to achieve the level of flood control described in the 1983 report, the North Detention Basin must limit the peak 100-year flow in Foss Creek at its crossing of Healdsburg Avenue to 100 cubic feet per second (cfs) or less. The 1987 General Plan calculated approximately 10 acres as being required for the construction of the basin to accommodate a storage capacity of 16.2 acre-feet.

The actual site requirements for the North Detention Basin, including storage capacity and minimum land area, will be based on updated hydrolic wetland/riparian habitat mitigation.

The North Detention Basin will provide improved flood control for portions of Foss Creek downstream of Healdsburg Avenue; benefit all of Area A and the remainder of the Northern Planning Area (Areas B and C) and downstream areas within the current City limits. The benefits include:

- Attenuation of peak flows in Foss Creek after development of Area A to considerably less than pre-development levels, thereby mitigating the downstream drainage impacts of development of Area A;
- Attenuation of peak flows in Foss Creek for Areas B and C after development of those areas to considerably less

than predevelopment levels, thereby mitigating the downstream drainage impacts of development of the portions of Areas B and C in the Foss Creek Watershed;

- Reduction of peak flows from the tributary portion of the Northern Planning Area to Foss Creek to levels significantly less than the current levels, even under undeveloped conditions.

The detention basin will be sized for a 100 year outflow not to exceed 100 cfs. This figure is considerably less than the 1-year storm flow in Foss Creek at this location before any new development. As a result, the North Detention Basin will attenuate post-development runoff from the portions of the Northern Planning Area within the Foss Creek watershed to less than pre-development levels not only for severe storms of recurrence intervals up to 100 years, but also for milder storms of recurrence intervals of less than one year.

Due to the outflow constraints during storms of even mild intensity, the North Detention Basin will likely be flooded several times per year, even in dry years, and more frequently in wet years. The Basin may also need to be fenced around its perimeter.

For optimum performance, it is necessary that the North Detention basin be constructed in the low area immediately upstream of Healdsburg Avenue. Alternative downstream locations are not considered feasible. The area downstream of the proposed location is substantially developed and severely constricted making the construction of the basin in this area technically difficult and expensive. Alternative locations further upstream are also considered undesirable due to decreased detention efficiency, increased land requirements and construction costs.

A number of features constrict the flow on the reach of Foss Creek downstream of Area A, including the roadway culvert at Chiquita Road, the foot bridge at Simi Winery and the railroad bridge between them. The hydraulic capacities of these structures are generally estimated to be adequate for storms with recurrence intervals of approximately 10 years under existing conditions of development, decreasing to 5 years or less under conditions of full development of the Northern Planning Area. Therefore, construction of the North Detention Basin should be begin before or concurrently with any new development in Area A.

The approximately 10 acres required for the construction of the North Detention Basin will be located in Area A. The basin would extend from the vicinity of Healdsburg Avenue on the west to the easterly limit shown in Figure 6A. The southerly limit would be established about 20 feet south of the most southerly top of bank of the T-75 Tributary or the southerly limit of any adjacent riparian vegetation. This would allow full access to the Creek channel for the construction of flow control structures. Providing 20 feet south of the Creek would allow for unconstricted access for maintenance which could share creek-side public access. Similarly, the northerly limit of the basin area would be established 20 feet north of the northerly top of the T-1 Tributary or any adjacent riparian vegetation. The area immediately upstream of Healdsburg Avenue below elevation 180 should be considered subject to flooding during severe storms (as presently occurs) and development should be restricted.

The depth of the basin will range from 4.5 feet to 10 feet at the top of berm surrounding the Basin. For these reasons, the Basin will have to be fenced.

### **Standards**

- a. Reserve a site in Area A for detention basin adequately sized based on an updated hydrological analysis and considering multiple use as public park land and wetland mitigation. If required, fencing should be aesthetically compatible with Area A's rural character. Fencing should be as open as possible so as not block views or call attention to itself.
- b. Design the Basin to avoid potentially hazardous currents.
- c. In the design of the Basin, employ curvilinear shapes and provide meandering slopes and banks to blend the Basin visually with the surrounding natural topography.
- d. To mitigate the impacts of increased runoff due to development of Area A, construct the entire North Detention

- Basin prior to or concurrently with the first development project.
- e. To minimize storm runoff, decrease runoff velocities and maximize the relief capacities of the detention basin and all stormwater drainage facilities:
    - i.) limit impervious surfaces in new construction;
    - ii.) maintain open drainage channels wherever practical
  - f. Design and construct drainageways and improvements to prevent stream bank sloughing during earthquakes.

### **10.1.2 WILDLIFE HABITAT AND EROSION**

#### ***Policy***

- Provide an effective network of creeks to manage stormwater while sustaining high-value, wildlife habitats and minimizing erosion.

#### ***Objective***

- Use open creek channels for drainage and to support riparian vegetation.

Area A's topography creates a network of creeks. In some of these areas, significant stands of riparian vegetation exist. Over time, riparian growth can be preserved and enhanced if creeks are left open and designed to permit percolation of water into the ground.

Erosion and siltation in creeks can occur as a result of construction and improper design of storm water drainage facilities. Erosion can destroy existing vegetation and siltation will impede new growth.

The frequency and severity of flooding downstream in Foss Creek can be mitigated by the detention basin at the confluence of the two major creeks crossing the Northern Planning Area at Healdsburg Avenue.

#### ***Standards***

- a. Keep all creeks in their natural, open channels wherever practical. Except for the Neighborhood Center, do not line or pipe drainageways except where crossing streets and roads.
- b. Maintain and enhance natural vegetation, especially tree cover along creeks as part of their design and/or to mitigate disturbance of existing riparian vegetation elsewhere within Area A.
- c. Minimize rerouting of natural drainageways where significant, mature riparian vegetation exists. Where rerouting occurs and where mature riparian vegetation does not exist, plant and/or relocate native riparian species at the top of creek bank but avoid planting brushy species within the channel that might interfere with drainage capacity or channel maintenance.
- d. Prohibit the siting of structures within 50 feet of the top of banks of major creeks and 25 feet of the top of banks of minor drainages.
- e. If creeks are to be maintained by the City, provide access points for maintenance equipment and trucks.

## **10.2 UTILITIES**

#### ***Policy***

- Extend utility services consistent with City standards as a condition of development approval.

#### ***Objective***

- Provide new residents in Area A with adequate utility services to maintain health safety and welfare.

## 10.2.1 SEWAGE AND WASTEWATER TREATMENT

The wastewater collection and treatment system in Healdsburg is owned and operated by the City. The system encompasses a network of collector lines of various sizes, eleven small pump stations in various locations throughout the City, a major pump station on Magnolia Drive, and treatment facilities on Foreman Lane, south of Dry Creek. In 1994, total dry weather flows into the sewer system averaged approximately 1.0 million gallons per day (mgd). Total wet weather peak flows, which include inflow and infiltration, have been estimated as high as 7.0 mgd.

All wastewater must pass through the pumping station on Magnolia Drive before reaching the treatment facilities. In 1984, the City increased the pumping plant's capacity and added an additional pressure main to the treatment facilities. However, during periods of severe rainfall, the capacity of the Magnolia Drive Pumping Station is marginal for existing flows. Current pumping capacity is approximately 6.5 million gallons per day (mgd) with all pumps operating. Based on information included in the "Pre-design Report for the City of Healdsburg, North Area Off-Site Sewer Improvements" dated November 8, 1991 by Brejle & Race, full development of Area A is anticipated to result in an increase in peak wet weather flows at the Station of approximately 0.4 mgd (approximately 6% of the existing peak flow), including inflow and infiltration.

It is currently anticipated that these improvements will occur in two stages, resulting in an ultimate pumping capacity sufficient to accommodate the combination of existing flows and additional flows resulting from full development within the City's ultimate limits. The first stage of these proposed improvements, the Phase I Magnolia Drive Pumping Station Expansion project, will involve the replacement of the existing 50 hp pump with a new 75 horsepower pump and an additional 75 hp pump. Upon completion of the Phase I project, total pumping capacity will be approximately 10.0 mgd with all pumps operating, and 8.2 mgd with any one pump out of service. The capacity should be sufficient to accommodate increased flows resulting from full development of Area A, in addition to existing flows, based on the assumption that development in Area A will precede additional development in other portions of the City.

The current hydraulic capacity of the City's sewage treatment plant is approximately 10.0 mgd with a current treatment capacity of approximately 1.4 mgd average dry weather flow. Existing average dry weather flow is approximately 1.0 mgd. The anticipated increase in average dry weather flow resulting from development in Area A is approximately 0.1 mgd. Existing treatment plant capacity should be adequate, to accommodate projected development in Area A, assuming that development of Area A will in general precede additional development in other portions of the City.

### Standards

- a. The aforementioned "Pre-design Report for the City of Healdsburg - North Area Off-Site Sewer Improvements", identified specific improvements required the portion of the City's sewer system between North Street and the southerly (downstream) limit of Area A to accommodate anticipated sewage flows from development of the Northern Planning Area, including Areas A, B and C. The report identifies a series of phased projects that must be implemented prior to any significant initial development and subsequent phases of development in Area A. However, the first three phases identified in the report will be implemented as one project due to funding and construction efficiencies.

Inclusive of the three phases identified in this report, the following improvements will be implemented prior to or concurrent with any significant initial development in Area A:

- i.) Construction of a new trunk sewer from the vicinity of Grant Street to Dry Creek Road. The southerly portion of this sewer would be approximately 21 inches in diameter and the northerly portion 18 inches in diameter.
- ii.) Construction of a new 15 inch diameter sewer to replace the existing parallel 8 inch and 10 inch lines in

Healdsburg Avenue from Dry Creek Road north to Foss Creek. This new sewer would be of sufficient size and buried at a sufficient depth (to cross under Foss Creek) to serve the full development needs of the Northern Planning Area.

At a later date, the additional improvement identified as Phase 4 in the report will be implemented. This would include replacement of the existing 18 inch sewer between West North Street and Grant Street with a new 24 inch sewer. However, this sewer improvement will not be necessary for the development of Area A unless concurrent development in the rest of the City greatly exceeds expectations. Upon completion of these improvements, all portions of the main trunk sewer system to the Northern Planning Area will provide sufficient capacity to accommodate full development of the Area.

- b. Within Area A, it is anticipated that a maximum of 2,500 linear feet of sewers proposed will have to be oversized to provide for future flows from Areas B and C. The sizes will vary from 8 inches to 12 inches depending on slope. (Figure 10B)

### **10.2.2 WATER**

The City owns and operates the major water system in the Urban Service Area that would serve the planning area. The City's water system includes two well fields adjoining the Russian River, several pumping stations, distribution lines, and a number of storage tanks and reservoirs. Based on utilization of the City's existing and proposed water rights to the Russian River and Dry Creek flows, it is anticipated that there will be adequate water supply available to serve the planning area. However, water demand generated from development within the City's planning area, a fish ladder project on the Russian River which threatens the reliability of the water supply of the Fitch Mountain Well Field, and recent determinations of the State Office of Drinking Water of seasonal non-compliance of the Gauntlett Well Field with regard to water clarity standards necessitate improvement and expansion of the water system to provide an adequate and high water quality supply to serve existing users and development from within the planning area. Therefore, an additional well field, adjoining Dry Creek at the site of the City Corporation Yard, is currently proposed.

Water service pressure available from the City's existing Gauntlett and Iverson storage reservoirs is adequate to serve all but a small portion of Area A. Approximately 6 acres, located on a ridge line in the central portion of Area A, are at or above an elevation of 320 feet. Above 320 feet in elevation, water pressure will be less than the minimum 40 pounds per square inch (psi). The installation of private individual booster pumps could alleviate low water pressure for dwellings that would be located above 320 feet in elevation. As only a very limited number of lots might be developed and their considerable distance from any other potential lots (including those in Areas A, B and C) at comparable elevations, the provision of a separate higher pressure zone for these 3 lots or their connection to a higher pressure zone at higher elevations would be economically impractical.

#### **Standards**

- a. Additional storage capacity at the Gauntlett Reservoir site is needed to accommodate development in Area A. As indicated in a previous report prepared for the City by Brelje & Race ("Operations Evaluation, Department of Public Works, Volume II, Water System", as amended January, 1992), the existing storage capacity at the Gauntlett site is approximately 270,000 gallons less than that required to serve current needs. Utilizing the evaluation criteria identified in detail in the report, the development of Area A will require approximately 800,000 gallons of additional storage capacity; therefore, it is recommended that a new storage reservoir of a least 1.0 million gallons capacity be constructed at or near the Gauntlett reservoir site. Land must be acquired for the reservoir. This new reservoir should be constructed before development of Area A exceeds 25% of the projected total or 130 units.
- b. Additional water production facilities are needed to adequately serve Area A. The Gauntlett Well Field, historically the source used to supply water to the Gauntlett and Iverson Reservoirs, has been determined to "...

not consistently meet the performance standards for surface water regulations." (Letter dated January 18, 1994 from the State Dept. of Health Services, Office of Drinking Water to the City Public Works Dept.) To provide water which meets drinking water standards both to the existing City and Area A, the City proposes to develop a well field at the site of the Corporation Yard adjacent to Dry Creek. The Dry Creek Well Field, is proposed by the City of Healdsburg to augment existing supply to serve the long-term growth of the City beyond the needs of Area A, improve reliability of the water supply, and make up for lost capacity and seasonal water quality problems at the City's existing well fields. This well field, if approved, is anticipated to be fully operational by 1996. If not approved, the City will likely install additional wells at its existing Fitch Mountain Well Field, and pump water taken from this well field to the storage reservoir serving the Gauntlett Service Area, which includes Area A, during periods when water from the Gauntlett Well Field does not meet State drinking water standards during the seasonable turbidity problems.

- c. On-site distribution lines within Area A are proposed to vary in size from 6 inches to 12 inches in diameter. Variation in line size will be required to provide efficient, reliable and resilient water distribution system for Area A, even when Area A is considered in the absence of any development in Areas B and C. The pipe sizes indicated, therefore, should not be downsized nor will sizes be required to be oversized to serve Areas B and C.

(Figure 10C)

### **10.2.3 ELECTRICITY**

Healdsburg owns and operates its own 12 kv electrical distribution system, obtaining power as a member of the Northern California Power Agency and a Preference Customer of the U.S. Department of Energy Western Area Power Administration. The Healdsburg electrical system is linked to the external power grid via a substation which is currently operating at about one-half capacity. The substation contains two transformers, each with a capacity of 25 megawatts. Generally, only one transformer is used with the other serving as a back-up. Peak usage is approximately 14.6 megawatts. Additional residential units, will generate a peak usage of approximately two to four kilowatts per unit.

Area A is currently served by Pacific Gas and Electric. With the development of the planning area, the City will assume service for Area A through the electrical distribution system it owns and operates. The City has existing overhead electric facilities along Healdsburg Avenue. Along the westerly boundary of Area A, these lines have limited capacity and are insufficient to provide service to Area A. The City plans to underground these facilities as part of Underground District No. 2. This work will be coordinated with the widening of Healdsburg Avenue as part of the Area A Assessment District. New electrical facilities for Area A will be installed underground as part of the Area A Assessment District in conjunction with Underground District No. 2.

#### **Standards**

- a. Over the last five years, the City has extended or upgraded 12 kv primary lines, of sufficient capacity to serve Area A, from its electrical substation to the southern boundary of Area A. This work was done as part of the City's Capital Improvement Program. Funds for this work were collected either through depreciation of existing assets or from fees collected through the City's standard development depreciation of existing assets or from fees collected through the City's standard development fee for electrical system improvements.
- b. From the southerly limit of Area A, in conjunction with Underground District No. 2, primary electrical service will be extended northward along Healdsburg Avenue to the main entry road for Area A. This portion of the primary electrical distribution system, as well as all electrical distribution facilities within Area A, will be installed underground. The costs of the primary electrical distribution system improvements along this portion of the frontage of Area A will be the responsibility of the developers of Area A. In order to minimize cost and disruption due to construction activities, the roadway improvements to Healdsburg Avenue (including new street lighting)

should be coordinated with the construction of the electrical system improvements.

Future electrical service to Area C will require the extension of the underground line in Healdsburg Avenue north of the main entry road. While the installation of electrical conductors is not required until the development of Area C, associated substructures, such as conduits and vaults, should be installed at the time of construction of the roadway improvements along Healdsburg Avenue.

From the intersection of the main entry road and Healdsburg Avenue, underground primary electric service will be extended easterly along the entry road to the easterly limit of Area A. Electric service for all of Area A will be provided by this line and the line in Healdsburg Avenue.

- c. An existing PG&E 60kv overhead transmission line through Area A will remain above ground; however the alignment will be shifted slightly with a new road that will service Area A and runs coterminous with a fault line running through Area A to Area C.

*(Figure 10D)*

- d. In addition to the primary service line along Healdsburg Avenue, provision of electrical service to Areas B and C and supplemental service to Area A will require the extension of a second primary distribution line from Powell Avenue, north through the easterly portion of Area A to Area C. Two alternative routes for this second primary distribution line are under consideration by the City Electrical Department.

The City's preferred alternative for the new line is to install it on the relocated PG&E transmission line poles. Since these poles will already be in place, the installation of an additional line on the same poles will have few additional impacts on Area A and the expense of underground installation would be avoided. While the installation of the second primary distribution line would not be required for Area A to initially proceed, it will be necessary for the City to reach agreement with PG&E when the existing overhead line is relocated to preserve the right of the City to install the second line at a later date and the relocated line's poles would have to be designed to accommodate the additional lines. The City and the developers of Area A should initiate discussions with PG&E regarding such an agreement at the earliest possible date.

In the absence of such an agreement with PG&E, an alternate underground route would have to be provided. As in the with the construction of electrical improvements simultaneously with the construction of improvements to Healdsburg Avenue, installation of conductors would not be required until development of Area C. However, in the absence of an agreement with PG&E regarding overhead installation, the necessary underground substructure for this line should be installed at the time of construction of the associated Area A roadways.

- e. The electrical distribution facilities proposed for construction to serve Area A (other than those identified to specifically serve Area B and C) are of the minimum size necessary to serve Area A and will not be need to be oversized.

#### **10.2.4 NATURAL GAS**

Pacific Gas and Electric provides natural gas to the City and will serve Area A in the future. Service is currently provided to existing residences in the southwest portion of Area A. Gas mains serving the planning area branch off a 3-inch low-pressure gas main situated along Healdsburg Avenue. Within Area A, there is one 2-inch low-pressure main located just north of the intersection of Healdsburg Avenue and Chiquita Road, and another along a shared private road in the southernmost portion of the planning area. A 12-inch high-pressure gas transmission line also runs along the western edge of the site, extending north from Chiquita Road. This line may not be buried deep enough to permit the changes to alignment, grading and widening anticipated for Healdsburg Avenue. If this is the case, the line will have to be buried deeper since there is insufficient land available to change the Healdsburg Avenue alignment.

#### **Standards**

- a. No constraints exist within Area A which are severe enough to limit extension of lines to the Area. Service would

- be provided by connecting to the 12-inch main. To accomplish this, a regulator station would be required.
- b. The cost of new and/or upgraded service to Area A will be determined by PG&E and passed on to the developers as a development fee. Reimbursement would be made by PG&E after customers begin purchasing gas.

### **10.2.5 TELECOMMUNICATIONS**

Pacific Bell has underground and overhead facilities that run along Healdsburg Avenue from south of Dry Creek Road to north of Alexander Valley road. The underground portions exist from south of Dry Creek Road to just north of Chiquita Road and from north of Hanning Road to Alexander Valley Road. Overhead facilities run from south of Dry Creek Road to north of Alexander Valley Road. In addition, there is an underground 50 conductor cable that runs across Area A from University Avenue to the intersection of Hanning Road and Alexander Valley road.

#### **Standards**

- a. Pacific Bell will serve Area A. No constraints are anticipated to extending telecommunication lines in Area A. To accommodate this additional area, the telephone company will have to upgrade the existing underground utilities from the central facility in downtown Healdsburg to just north of Chiquita Road. This work may require new duct work or insertion of additional conductors into the existing duct work, depending on the existing duct configuration. In either case, Pacific Bell would pay for the upgrade.
- b. Pacific Bell will provide lines without cost for new development within a trench, if the trench is provided to them. A fee will be charged for extending lines between the trench and new residential buildings. An additional fee will be charged for undergrounding existing overhead lines within Area A. The City requires undergrounding of lines and stipulates that associated costs are the responsibility of individual developers or property owners.

### **10.2.6 CABLE TELEVISION SERVICE**

#### **Standard**

Cable television service will be provided by Viacom Cablevision by extending new underground facilities along on Healdsburg Avenue from the existing overhead facilities at Terrace Boulevard just past Chiquita Avenue. Expansion of cable services usually proceeds concurrently with work done by the City to provide new service to areas or when their existing facilities are relocated. The City provides notice to the cable company when these service changes will take place so that the cable extensions can begin. Costs of expansion would be paid for by the developer of Area A.

## **10.3 FIRE**

### **10.3.1 FIRE AND EMERGENCY SERVICES**

#### **Policy**

- Provide fire and emergency services to Area A and the entire Northern Planning Area consistent with City standards.

#### **Objective**

- Provide fire and emergency services must be adequate to serve existing and future residents.

#### **Standard**

The Healdsburg Fire Department will assume primary responsibility for Area A and the entire Northern Planning Area

as this area is annexed to the City. The City currently maintains an overall insurance rating of 4 on a scale of 1 to 10, with 1 being the best rating. Response times for medical and fire emergencies average 3 minutes within most of the city limits, however, response times to the far north area and other parts of the Urban Service Area are longer.

Response time to the planning area is the greatest concern related to fire service. The northeastern and southeastern portions of the northern planning area, including the easternmost portion of Area A, have been rated as a high fire hazard area by the Healdsburg Fire Department. As development occurs in the northern planning area, there may be increasing pressure to develop a second fire station in the Northern Planning Area.

The findings and recommendations of the "Facility Options Report" undertaken by the City in 1989 determined that the construction of a centralized facility and a new substation in the northern section of the City in 10-15 years would be the most effective at meeting the department's long term needs. A larger, centralized facility was subsequently built at the intersection of Healdsburg Avenue and Grant Street, affording better access and faster response time to the Northern Planning Area. It has been determined that if a new substation is eventually needed for the northern section of the City, it will be located in Area C.

### **10.3.2 WILDFIRE PROTECTION**

#### **Policy**

- Increase protection of development from the threat of wildland fires.

#### **Objective**

- Decrease fire hazards to new development through open space planning and design of streets.

Area A contains areas of high wildland fire hazard, some of which lie within Area A's ridgeland areas. These areas are prone to fires because of their steep topography and the availability of highly flammable chaparral vegetation. Consistent with General Plan goals and policies (Part I, Section VII, p. 61), the Plan reserves areas of high fire hazard for open space and low intensity uses, and provides adequately designed streets to provide convenient emergency access to all areas.

#### **Standard**

- a. Create a fuel break along the perimeter of development adjacent to high wildland fire areas. The location of the fuel break shall be coterminous with the north/south roadway running from Area B to the entry road for Area A. In this way cutting or disturbance of vegetation will be kept to a minimum as necessary to construct the road. The fuel break shall be at least 100 feet in width and be accessible from public streets by fire fighting equipment via a passable right-of-way of at least 12 feet. The City's Fire Department shall determine the proper treatment of vegetation within this area. Methods include clearing vegetation, pruning trees, and planting fire-resistant ornamental shrubbery and groundcovers.
- b. Per City requirements, equip all new construction with automatic sprinkler systems and fire-retardant roof coverings.
- c. Require all plans for new development be reviewed by the Fire Department to ensure adequate access, appropriate placement of hydrants, and adequate fire flow rates.
- d. In all new development in high fire hazard areas, provide for a clearance around structures and the use of fire-resistant groundcovers and shrubbery.

(Figure 10E)

## **10.4 POLICE**

### **Policy**

- Provide police protection services to Area A.

### **Objective**

- The health, safety and welfare of existing and future residents of Area A shall be maintained by the provision of adequate police services.

### **Standard**

Provide adequate access to newly developed areas and maintain high levels of service at major intersections on Healdsburg Avenue to maintain the high level of response time for police services.

## **10.5 SCHOOLS**

### **Policy**

- Provide for future school needs of residents in the Northern Planning Area.

### **Objective**

- A new elementary school site should be planned in anticipation of future school needs in the Northern Planning Area.

The extent to which planned and existing schools will meet future needs is unclear and under study. The two older elementary schools within the Healdsburg Union School District are both operating at more than their design capacity. A third school was recently completed on the west side of Healdsburg Avenue, south of Chiquita Road. Development in the Northern Planning Area, at buildout, is projected to generate approximately 350 elementary school students, requiring approximately one elementary school (By itself, Area A would generate approximately 160 elementary students). A potential elementary school site is therefore being proposed for the Northern Planning Area.

### **Standards**

- a. Provide 10 acres of flat land for an elementary school in the Northern Planning Area. The site shall be located in Area C.
- b. Provide 10 acres of land for a community park adjacent to the school site to permit shared Park and School facilities.
- c. Ensure safe pedestrian access to the school site by providing appropriate trails, paths, sidewalks, and street crossings.
- d. Provide land uses adjacent to the school which will be compatible with an elementary school.
- e. Mitigate school impacts created by residential development to the extent legally permissible, including by participation in a school mitigation program acceptable to the City of Healdsburg.

# **11. COMMUNITY DESIGN ELEMENT**

# 11 COMMUNITY DESIGN

The Urban Design Element sets forth policies, objectives and standards for the siting and design of development.

## 11.1 CULTURAL CHARACTER

Many features in the City of Healdsburg contribute to its unique character and sense of place. These features can serve as a model for new development in the Northern Planning Area to extend Healdsburg's character into the new development areas. These features include the town's street grid, its access to surrounding open space, and its architectural character. By designing a new development pattern that uses elements of the existing development pattern of the City of Healdsburg, the perception that a new town is being created will be minimized and continuity with existing development can be maximized.

In that portion of Area A where flat land is concentrated and few environmental constraints presently exist, a grid pattern of streets will be used in the Northern Planning Area to create a recognizable, cohesive and traditional framework around which new development will be organized. The grid offers many ways to move from one place to another, while providing an easily understandable system for recognizing one's location and direction of travel within the planning area, unlike the cul-de-sac streets that characterize so much suburban development today, which are both confusing and inefficient.

The grid pattern is not appropriate for those portions of Area A that contain steep slopes, sensitive vegetation or other environmental constraints where imposition of a grid pattern would impact these resources. In the remaining portions of Area A outside the flat valley to the north, streets would be designed and laid out to respect topography, vegetation, soils, etc.

Healdsburg's older neighborhoods contain a well-maintained stock of traditional, northern Californian residential structures from several historic periods. These neighborhoods with their mature landscape and variety of design have considerable charm. Within a generally single-family context, the older neighborhoods also provide a surprising variety of housing densities. Garages on the older homes also tend to be situated back from the street leaving room for yards and generous entrances to dwellings.

Furthermore, the simple architecture and construction of the older homes creates a continuity and elegance in the neighborhood. Most houses, while varying in size, are fundamentally the same—a well-proportioned woodframe box combined with basic elements such as steps, porch, bay window, cornice, and trim. It is the variety with which these elements are combined that contributes to the City's identifiable and pleasing character.

*(Figure 11A)*

Another important aspect of Healdsburg's cultural/aesthetic environment is the proximity and access to open space. Nearby rural areas provide a frequent reminder of Healdsburg's small size and historic link to agriculture. Similar relationships with the natural surroundings can be created in Area A by preserving the area's natural systems and habitats, maintaining the area's scenic, rural qualities, and providing physical and visual access to open space areas with trails and visual corridors.

The natural topography of the Northern Planning Area will be used to create a distinct and recognizable edge to the urbanized area, avoiding the appearance of sprawl by confining high density development to the flatter, more suitable development areas. Consequently, the Northern Planning Area will become Healdsburg's northern gateway, establishing the image and identity for this important entrance into the City.



A. Passalacqua House  
321 Haydon Street  
Transitional Queen Anne  
Built: 1912



Samuel Meyer House  
219 North Street  
Italianate  
Built: 1882



George Alexander House  
423 Matheson Street  
Queen Anne  
Built: 1905



John King House  
619 Johnson Street  
Italianate  
Built: 1885

**HISTORIC HEALDSBURG HOMES**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

Wallace Roberts & Todd  
Jones & Stokes Associates

Source:  
Historic Homes of Healdsburg  
A Self Guided Tour

## **11.2 SITING OF DEVELOPMENT**

### **11.2.1 SITING OF DEVELOPMENT**

#### **Policies**

- Prohibit development in valuable habitats, including riparian woodland, natural wetlands, and serpentine chaparral by ensuring that proposed building sites, improvements and access roads are located outside of these areas during the tentative map or planned development approval process. Minor encroachments may be permitted by the Healdsburg Public Works Director where it can be demonstrated that a significant impact will not occur (with or without mitigation).
- Alternatively, protect valuable habitats within publicly or privately owned and maintained open space preserves.
- Limit development in areas with geotechnical instability or valuable habitats.

#### **Objective**

- Establish specific criteria for the siting of new development to mitigate environmental impacts.

Area A contains a complex set of environmental conditions that must be addressed before development may occur, including geotechnical, biological, aesthetic and archeological concerns. These siting standards describe the criteria for determining suitable locations for structures, yards, driveways and certain activities. These criteria are intended to provide a checklist to be used by developers to guide the siting and design of development in response to these environmental factors and by City staff during the development review and approval process to determine compliance with the plan. The approximate locations of environmentally "sensitive" areas (i.e., high value habitat, historic features) and "constrained" areas (i.e., landslide areas, fault zones, corridor along Healdsburg Avenue with unacceptable noise levels) where siting of new development may be restricted are illustrated in Figure 11B. As development plans are submitted for review by the City, additional information may result in adjustments in the location of environmentally sensitive or constrained areas where development is restricted.

*(Figure 11B)*

#### **Standards**

The following standards shall apply to development within sensitive areas as indicated in the Plan.

- a. On properties containing environmentally sensitive areas designated by the plan, maintain development, including roads, driveways, gardens and housing, outside the boundaries of such areas. Properties where development siting is constrained are indicated in Figure 11B. The precise location of environmentally sensitive or constrained areas relative to proposed lot lines, building sites and access roads shall be determined or adjusted on a project by project basis at the tentative map stage as part of the required Initial Study, based on more detailed site specific mapping and/or additional technical studies to be submitted by project applicants.
- b. To avoid environmentally sensitive areas and maximize use of less constrained areas, clustering and variable lot size is encouraged. In order to facilitate clustering and variable size lots, property owners and developers may elect to use a Development Cluster Overlay Zone in the environmentally sensitive Ridglands portion of Area A. This option will permit clustering of lots by allowing lots smaller than otherwise allowed under the base zoning, as long as the overall maximum density is not exceeded. In order to encourage use of clustering, and in recognition that more housing may be built in some areas if clustering is used while still respecting environmental constraints, a slightly higher density (1.3 dwelling units per acre) will be allowed than would otherwise be allowed under the base zone.

- c. Development shall be clustered to avoid environmentally sensitive or constrained areas, and areas with high value wildlife habitat or significant natural vegetation. Environmentally sensitive and constrained areas shall be located within the acreages of larger lots away from building sites and required improvements, or located in a common open space lot and maintained to preserve sensitive natural features.
- d. In the Neighborhood Center and Ridgeland, the siting of development will be constrained by earthquake faults. A minimum setback of 50 feet shall be established and maintained, or as approved by the City's geotechnical consultant.
- e. In the Ridgeland, roads, driveways, and building sites shall be located outside of landslide areas, as indicated on Figure 11B. Site specific geotechnical, vegetative, or other investigations as determined by the City may be required and used to adjust the location of these indicated landslide areas, and exceptions can be made for situations where landslide risk can be shown to be fully mitigated by engineering techniques.
- f. In the Neighborhood Extension, Ridgeland and Gateway areas, all habitable structures shall be setback a minimum of 170 feet from centerline of Healdsburg Avenue to protect against noise impacts. Within this setback, development may be proposed if it is determined on the basis of an acoustical analysis that methodologies such as the proposed setback from Healdsburg Avenue and/or the use of berms, landscaped sound walls or non-habitable structures, will adequately buffer noise to meet the General plan interior noise level standard. Sound walls are to be avoided unless no other means of buffering is practical. Where sound walls are used, landscaping shall be used to screen walls or cover with vines, in order to improve the appearance of these walls and discourage graffiti.
- g. Roads, driveways, and buildings shall be sited to avoid existing historical sites or features unless such sites or features are determined by the Healdsburg Public Works Director to be not significant or adequate mitigation is proposed.
- h. The location of lots and building sites shall be planned in connection with the location of required fuel breaks to minimize the need to remove significant natural vegetation (e.g. large trees, woodlands).

### **11.3 GRADING**

#### **Policy**

- Grading shall respect and emulate the natural topography by blending with the surrounding terrain, and shall not contribute to erosion.

#### **Objective**

- Grading should protect important topographic features containing sensitive natural resources and scenic qualities.

Area A contains important topographic features which comprise sensitive natural resources and contribute to the rural qualities and beauty of the area.

Where grading must occur for development, standards shall be met to assure that regrading of sites is done in a manner that takes into account the visual and environmental impact of new development within the larger context of the landscape of Area A and the City and that graded areas are revegetated.

#### **Standards**

- a. Minimize grading for new construction by:
  - locating structures and parking in flatter areas;

- minimizing the use of flat pad construction techniques and the size of any graded level pads;
  - stepping structures up grades with small retaining walls
  - using post and pier foundation systems to raise structures over steep terrain
  - aligning driveway rights-of-way with contour lines and limiting the width of the driveway
- b. Cut and fill slopes shall be no steeper than 3:1 on average; and retaining walls shall not exceed 4 feet in height unless an integral part of a structure's foundation. If part of a foundation, retaining walls shall not exceed 9 feet in height.  
(*Illustration 1*)
- c.. Maintain the character of the natural terrain by varying gradients and undulating contours of graded areas to blend with the existing landform. In the transition areas between natural and graded areas, harsh graded edges should be rounded to resemble natural terrain. The toe and crest of any slope greater than 10 feet in height shall be rounded with vertical curves of at least 5 feet in radius.  
(*Illustrations 2, 3 and 4*)

## **11.4 DESIGN OF DEVELOPMENT**

The following guidelines have been created to guide the design of new development. The guidelines stem from two design policies:

- to maintain and enhance the traditional and rural characteristics of Healdsburg and its surroundings
- to create a distinctive, high-quality environment.

### **Policy**

- Recognize the physical and environmental differences of areas within Area A, by dividing the area into sub-areas, each with its own design guidelines. As stated before these sub-areas are the Neighborhood Center, the Ridgeland, the Gateway and the Neighborhood Extension.

### **Objectives**

- Establish design guidelines to address physical design issues such as size, height, setbacks, massing, architectural features, and parking. Consistent application of these guidelines is necessary if new development is to establish the intended character of each sub-area.

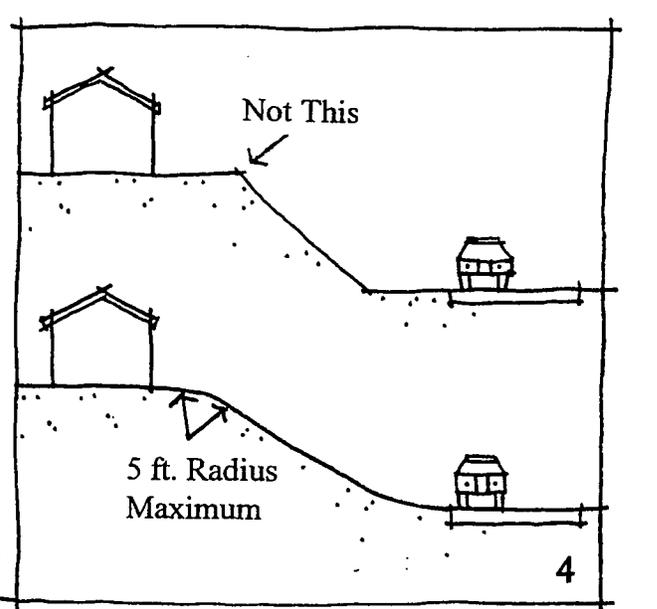
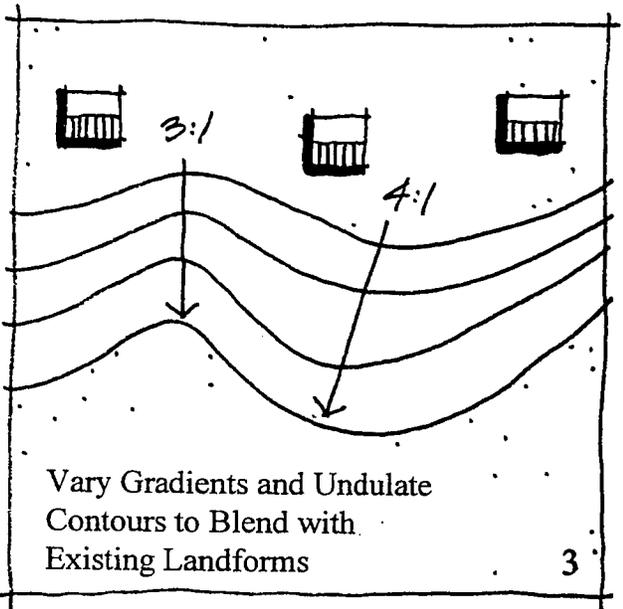
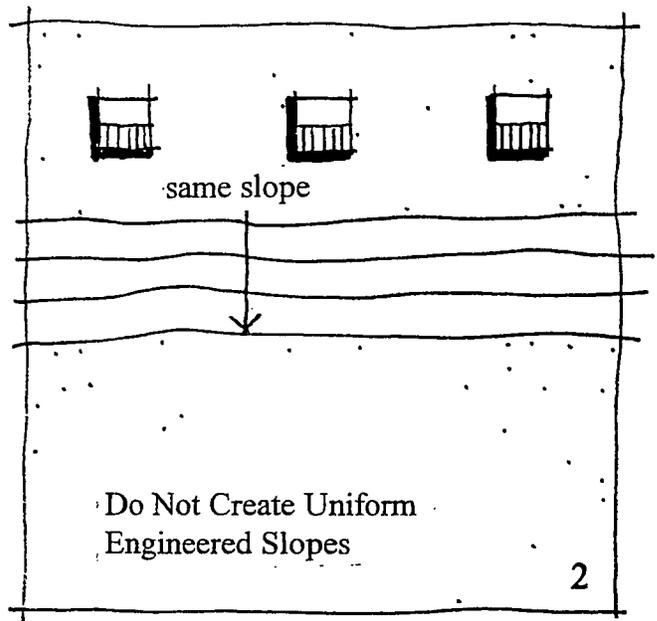
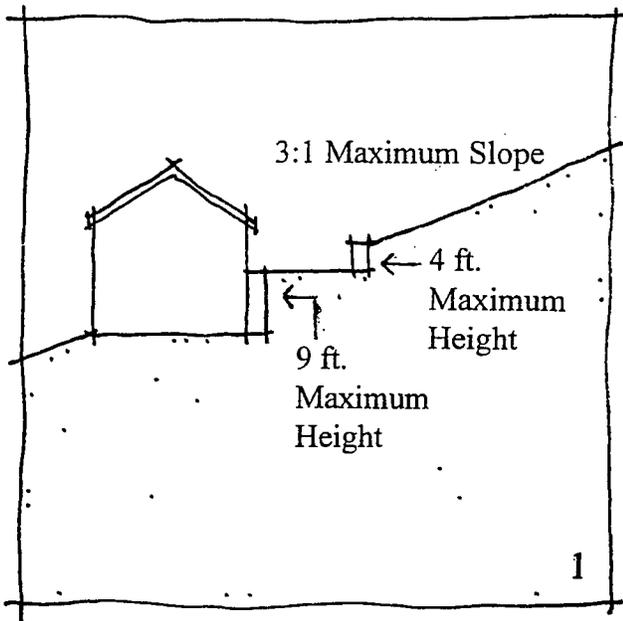
### **11.4.1 THE NEIGHBORHOOD CENTER**

#### **Policy**

- Establish the Neighborhood Center as a pedestrian-oriented neighborhood located in the flatter valley area in the north end of Area A. Streets and parcels will be organized within a grid pattern similar to that found in historic Healdsburg.

#### **Objectives**

- Utilize a grid pattern of streets as the organizing element for the Neighborhood Center. As in historic Healdsburg, homes will clearly face the street with front yards, porches and entries.
- In building design, reflect the traditional character of Healdsburg's historic architecture. Building volumes and roofs should be simple, contributing to the Neighborhood Center's cohesive image. Porches and bays will punctuate and enliven the streetscape, as will materials and architectural details.



## ILLUSTRATIONS

### NORTHERN HEALDSBURG SPECIFIC PLAN

Area A

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## **Standards**

### a. Development Pattern

The grid pattern of streets shall be created to provide circulation through and within the Neighborhood Center. The grid shall be extended to Area C in future plans. With the exception of apartments permitted under the provisions of the AH-1 overlay zoning district, all dwelling units shall be oriented toward streets. If possible, parking should be placed at the rear of lots except where constrained by topography or drainageways.

### b. Dwelling Unit Types

The following unit types shall be permitted:

- Single Family Detached (one unit)
- A second unit may also be constructed at the same time as the single family residence on lots of at least 6000 square feet in area, but must comply with these development standards and applicable zoning restrictions.
- Any unit type is permitted under the provisions of the Affordable Housing Overlay Zoning District 1 or 2 where this district is established.

*(Illustration 5)*

### c. Density

Density bonuses shall be permitted by either state law or the City's Affordable Housing Overlay Zoning District 2 (AH-2).

### d. Lot Configurations and Sizes

The character of the Neighborhood Center will be dependent on providing diversity in lot sizes and configurations as well as diversity and human scale in the design of new structures. Large projects often make streets less lively and personable due to the repetition of identical massing and architectural details. This repetition tends to exaggerate the perception of the project's size.

The relationship of the size of a dwelling with the size of the lot is very important in establishing the scale and character of development.

The total square footage of a building footprint shall not exceed 50 percent of the total lot area. (Note: This includes main and accessory structures on the site. Excluded are driveways, decks and patios which may be counted toward open space requirements.)

*(Illustration 6)*

Lots widths measured at the front and rear yards shall not be less than 40 feet.

Healdsburg's historic areas contain a diversity of lot sizes and housing which provide visual interest and avoid uniformity and repetition. To provide this diversity in the neighborhood center of Area A, a variety of lot configurations and lot sizes are required for projects larger than three acres in size. Lot configurations may include, but are not limited to, zero lot line lots, angled Z lots, zipper lots, alternative width lots, quad lots, and motor court lots. Lot sizes may not be less than 3,500 square feet.

### e. Diversity in Architectural Design

To provide visual interest and diversity, no unit type shall be used more than twice within the same block without altering architectural features.

### f. Building Height

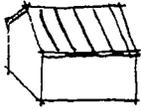
Consistency in building heights will contribute to the Neighborhood Center's identity. Heights greater than one-story will establish a distinctive profile and better enclose and define the open space created by streets. In addition, two-story heights provide more housing opportunities by permitting various unit densities.

Two-story houses are strongly encouraged in this subarea.

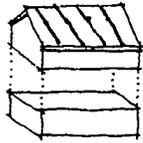
### g. Yards

Consistent building setbacks from streets (i.e., front yards), are important in providing the scale appropriate among

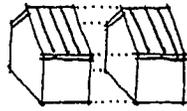
Single-Family  
Detached



Two Plex

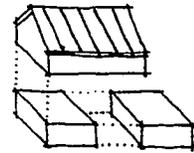
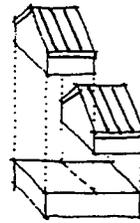


over/under

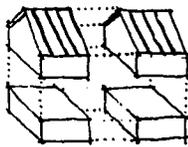


side/side

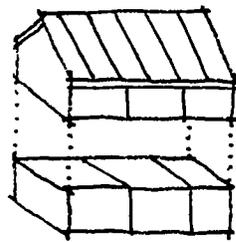
Three-Plex



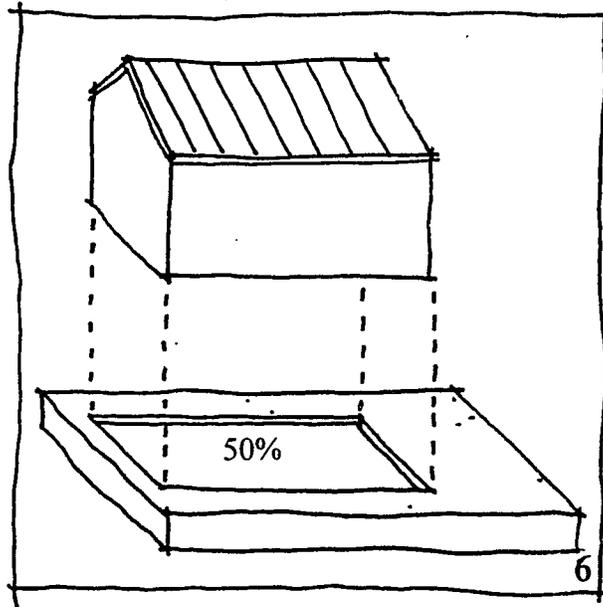
Four-Plex



Apartment



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**ILLUSTRATIONS**  
NORTHERN HEALDSBURG SPECIFIC PLAN  
Area A

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buildings in a residential neighborhood by visually and physically separating structures to avoid the appearance of crowding. Together, the front yard and its landscaping greatly contribute to strong, coherent neighborhood identity and pleasant enclosure of the street. The yard also provides opportunities to establish a transition between public sidewalks and private interiors, which encourages activity and enlivens the street. In addition to front yard setbacks, all dwelling units will have private usable open space.

For housing projects constructed within AH-I overlay zoning districts, setback and yard requirements set forth in the development standards of the zoning ordinance for this district, shall take precedence over the setback and yard requirements stated below.

i.) Front Yards

Dwelling units shall have a minimum front yard of 10 feet, measured from the back of the sidewalk. In no case shall a residential building in the Neighborhood Center have a front yard greater than 25 feet in depth. Whether garages are attached or detached from residential buildings, it shall be setback at least 20 feet from the back of the sidewalk so vehicles will not overhang the sidewalk.

Buildings on corner lots shall have two front yards along each street frontage.

Hedges, retaining walls low, fences (36" maximum height) or a sloped bank are encouraged along the front property line to separate public sidewalks and front yards.

*(Illustration 7)*

ii.) Private and Common Open Space

All single family lots shall provide a minimum of 400 square feet of usable private open space. For all other units, the minimum amount of open space provided per unit shall be 300 square feet, which may be a combination of private open space and a common yard area which is accessible to all units on the lot.

h. Garages

Garage doors are encouraged to be equipped with roll up doors rather than tilt up doors to enhance appearance of neighborhood. With the exception of any off-street parking structures or areas provided for apartments permitted under the AH-I overlay zoning district, which need only to comply with appropriate provisions of the City's Design Review manual, garages shall also comply with at least one of the following requirements:

- the garage door shall face to the side street, if one exists.
- the garage shall be recessed at least 4 feet behind the facade of the building when the garage faces the street.

i. Dwelling Unit Entries

The main entries to residential units and their associated porches and landings, are important features in small towns, encouraging community activity while also enhancing interior privacy.

- i.) Residential buildings shall have at least one entry which provides a direct physical and visual link to the street. Separate entries which provide direct access to the street or the outside of the building are encouraged for all units when practical for the unit type.

*(Illustration 8)*

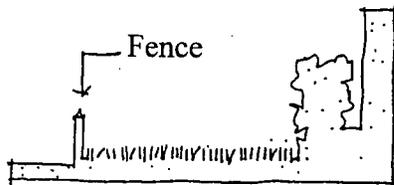
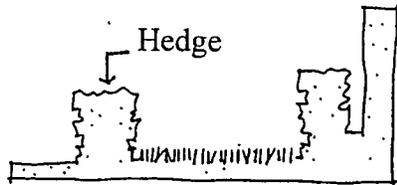
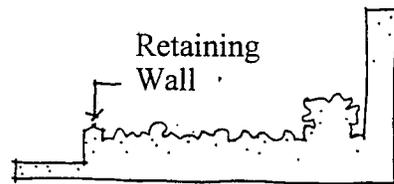
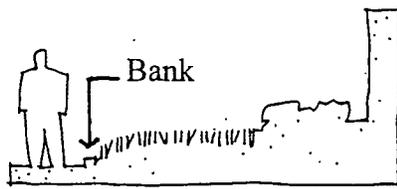
ii) Covered entry steps, porches, covered ramps and landings are encouraged.

iii) Accessibility for those with limited mobility is encouraged through use of ramps or alternative entrances without curbs or steps.

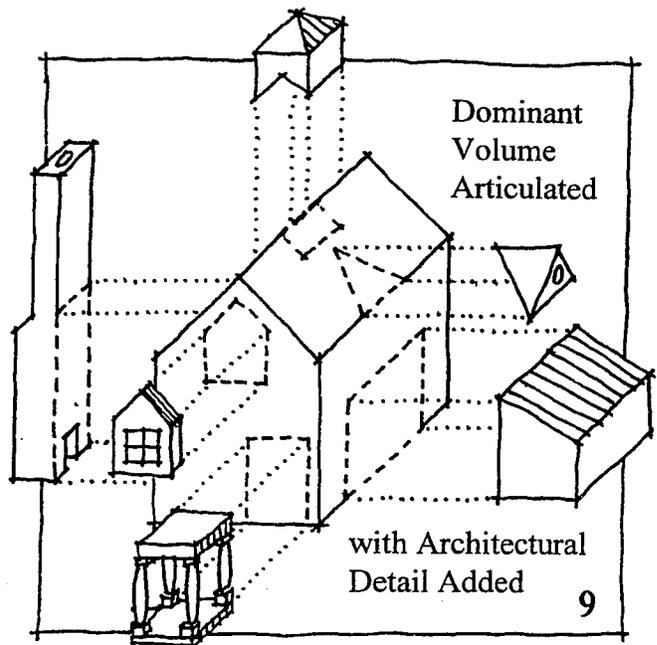
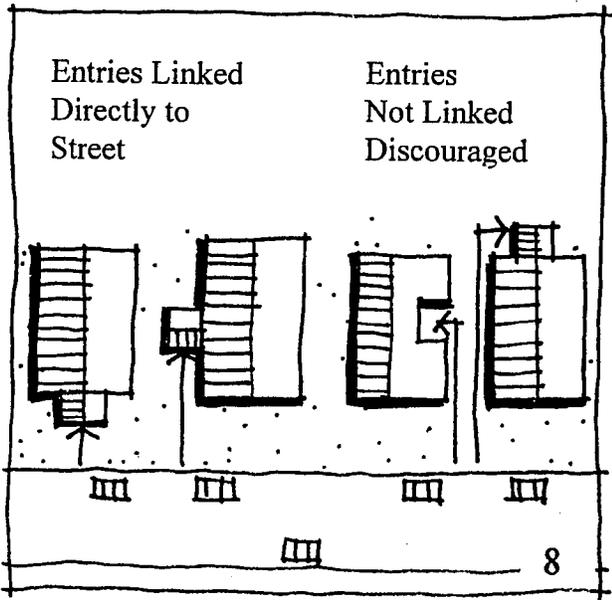
j. Massing

In Healdsburg and other small northern California towns, the traditional form for buildings is a fairly simple box form as the primary volume which is then articulated with smaller architectural details. This articulated box form gained prominence because it is relatively simple to construct.

Especially in the traditional single-family detached house, the main volume is always legible and is not compromised by complex or competing secondary volumes and angles that do not clearly relate. Thus, the traditional house provides a consistent, basic form that when repeated throughout a neighborhood contributes to the



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**ILLUSTRATIONS**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
 Area A

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neighborhood's coherence. Diversity is created by varying architectural features, details and ornaments.

- i.) Buildings designed with one dominant volume or mass, articulated with smaller add-on masses such as bay windows, dormers, porches, trellises, chimneys and cupolas are encouraged.
- ii.) Architectural details that establish a human scale are encouraged. Opportunities for ornamental details include: columns, brackets, foundation skirts, vents, rain spouts, windows, shutters and doors.

(Illustration 9)

k. Roofs

The form of roofs and their materials contribute to the image and identity of a neighborhood. The silhouette made by many roofs may be a neighborhood's strongest feature when seen from a distance. Close up, the shape and materials of roofs help define the street enclosure. In northern Californian towns, where winter rains can be heavy, most roofs are hipped or gabled, and made of metal or shingles.

- i.) Roofs shall be hipped or gabled; not flat.
- ii.) Roofs made of shingles, flat tiles or metal are permitted.

(Figure 10)

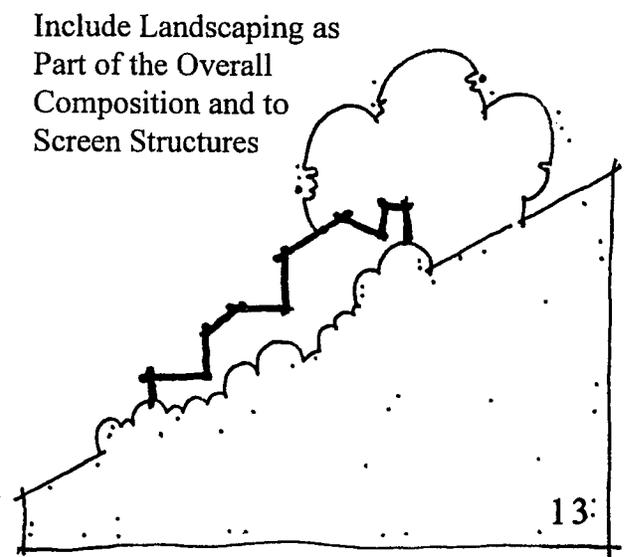
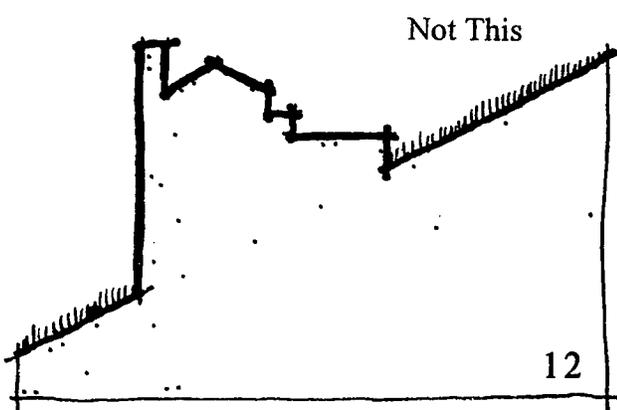
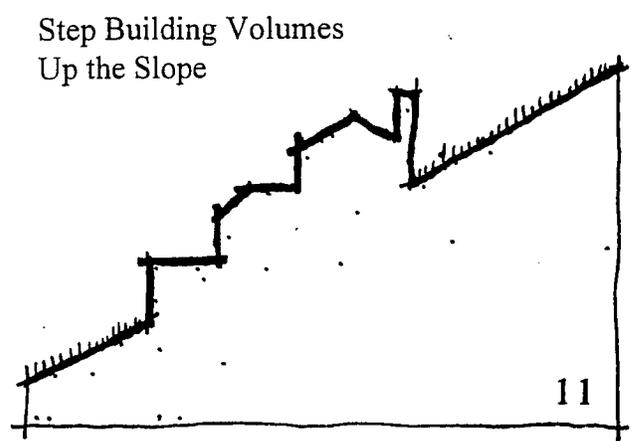
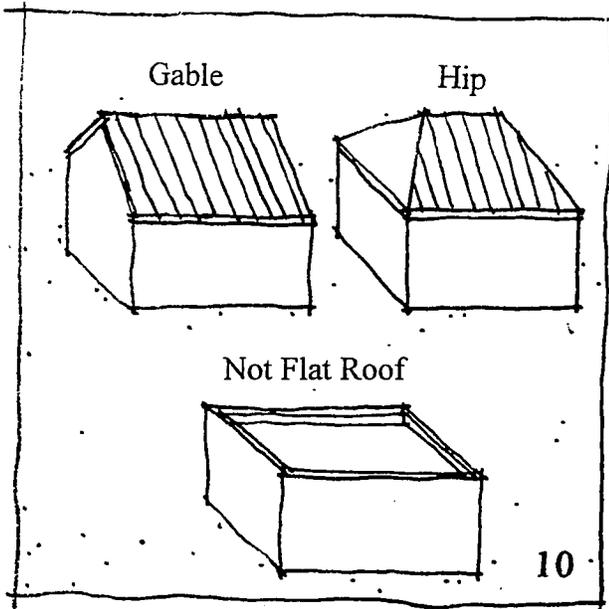
l. Exterior Finishes and Fenestration

Consistent use of exterior fenestration; facade and roofing materials; and colors contribute to a coherent small town image. Exterior finishes should convey a residential image through articulation and scale. Inset (rather than flush) windows and wood window frames create more variety and richness in details and shadows. Finally, lighter colors enliven building facades by emphasizing shadows from cornices, eaves, siding and other architectural features.

- i) Exterior finishes must be articulated with the massing, scale and detail of the building to create a residential image and identity.
- ii) Windows that are inset and have frames are encouraged.
- iii) Light exterior colors are encouraged.

**Summary of Required Dimensions - The Neighborhood Center**

Minimum Lot Width (at the front face of building)	40 feet
Maximum Lot Coverage	
Footprint	50% of lot.
Maximum Building Heights	2 Stories
Minimum Front Yard (except AH-I Units)	10 feet
Minimum Private Yard Area	
Single family	400 sq. ft.
Minimum Open Space Area	
Multi-unit projects (per unit)	300 sq. ft.
Minimum Setback for Garages from Street	
Right-of-Way (attached or detached)	20 feet
Minimum lot size	3,500 sq. ft.



**ILLUSTRATIONS**  
**NORTHERN HEALDSBURG SPECIFIC PLAN**  
 Area A

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## 11.4.2 RIDGELANDS

### Policy

- Create a sub-area in Area A that recognizes the greatest concentration of sensitive natural resources. In order to retain and preserve the sensitive environmental features in this sub-area, the Ridgeland will be designated for either detached, single-family homes on relatively large lots, or clustered development where variable lot size and any housing type will be permitted. Preservation of open space will be accomplished primarily using privately owned and maintained common lands maintained through a homeowners association or a landscape and lighting district, or areas off-limits to development within acreages of individual private properties. Where deed restrictions and permanent easements may be used to limit disturbance of the land by restricting development and prohibiting future subdivision.
- Use design guidelines to make new development less visually apparent. Development will blend with the existing natural features through careful siting, grading and building design. Yards, fences and walls will meet private needs, while minimizing intrusion into sensitive environmental areas. Design guidelines and standards for materials and construction will further reinforce the Ridgeland's rural character.

### Standards

- a. Development Pattern  
Development should be designed and located to best maintain the rural character of the area. Where possible, buildings should either be clustered or widely separated (rather than be regularly spaced along roads), and use topography, vegetation and materials to screen buildings from roads, distant views from other parts of the City and from each other. While minimizing the visual presence of development in the area, all new construction shall preserve areas with significant environmental constraints. Where severe environmental constraints exist, clustering of development is encouraged to minimize disruption of the land. On scenic ridgeland buffers (identified in Figure 3F, and where a visibility analysis is required by the city General Plan Policy Document), these additional standards shall apply:
  - i.) Structures shall be designed and located, and grading and other construction activities shall be performed so that no trees over 20 feet tall are removed or disturbed. If removal or disturbance of such trees is found to be necessary, replacement is required, using tree species native to the local area with minimum height and spread of 12 and 8 feet, respectively, at a minimum replacement ratio of 3:1.
  - ii.) Only developments that are shown to be unobtrusive based on the visibility analysis may be approved. Structural projections above the ridgeline shall not be allowed unless it can be demonstrated that the projection will be screened by, or be subordinate in scale to, existing natural features.
- b. Yards, Setbacks and Clustering  
Where topography and vegetation permit, generous yards or common lands held in open space will reinforce the rural identity of the Ridgeland.
  - i.) Residential buildings shall have a minimum setback of 30 feet from all property lines, except in clustered development or areas where this requirement would destroy high value habitat or necessitate undesirable grading. In these instances, setbacks shall be determined at the time of subdivision based on an environmental analysis of the site and the constraints contained therein. Goals relating to geologic hazards, biologic resources, aesthetic factors and cultural features shall be enforced and shall be used to determine the appropriate setback where the minimum standard of 30 feet cannot be met.
  - ii.) Clustering of development may be permitted in these cases. However, in no case shall the front yard be less than 20 feet or the distance between buildings with attached units be less than 20 feet.

c. Plant Materials

- i.) In the immediate zone surrounding structures, fire resistant landscaping is encouraged. However, the use of native and indigenous plant materials only is encouraged in areas away from structures and where native woodlands and grasslands predominate. In such areas, new plant materials shall be planted a naturalistic manner.
- ii.) Within high wild fire hazard areas, significant amounts of flammable vegetation and combustible materials shall be removed within 30 feet of residences. This does not pertain to pruned trees, specimen trees, or ornamental shrubs and groundcovers.

d. Parking and Garages

Many rural residences have detached garages or garage entrances located in back of the house, away from the road, leaving the front yard free for yards, landscaping and generous entrances. Placing the garage in front of the house can reduce front yard area and replace residential building facades containing entries, bays and windows with large, inarticulated, and monotonous garage doors.

- i.) Garages shall be set back at least 30 feet from property lines except where environmental constraints would restrict the siting of the garage. To protect sensitive natural resources, garages may need to be located close to the road. Where the garage is located on a slope that falls away from the structure, the garage shall be setback from the street a minimum of 20 feet, so that parked cars do not overhang into the street.
- ii.) Where the garage is located on an uphill slope, the setback shall be determined given the existing conditions on the lot. Detailed drawing shall be presented to the City to show the location of the garage and how the proposed location will minimize grading, disturbance of vegetation and the location of any retaining walls. In no case shall the garage be setback less than 10 feet from street right-of-way nor shall a retaining wall greater than 9 feet be created.

e. Massing

The basic form, volumes and height of a structure can reinforce or detract from the natural landforms.

- i.) Encourage variation of building volumes and massing to keep the building's features in similar scale with surrounding natural features.
- ii.) Step the building's volumes to match the natural slope, thereby accentuating the natural topography and reducing the need for grading.
- iii.) Include landscaping as part of the overall composition of structures. Use taller trees as a visual backdrop; and create a natural base with shrubbery.

*(Illustrations 11, 12, and 13)*

f. Roofs

The form of roofs and their materials can contribute to the rural image of the area. Hip and gable roofs are predominant in the area and should be encouraged on new construction. (See Neighborhood Center) City code requires fire resistant roofing materials in areas with high fire hazard potential, including the Ridgeland portion of Area A.

g. Entries

Traditional rural dwelling units typically have distinctive, formal entrances. Many contain porches and porticos, and are characterized by their inviting appearance.

Encourage the creation of distinctive and inviting entries with features such as entry steps, porches, and porticos.

h. Materials and Construction

The color and texture of a building affect its ability to blend visually with the natural surroundings.

- i.) Structures and fencing should have darker, neutral colors.
- ii.) Portions of the Ridgeland lie within high hazard wild fire areas. Fire resistant materials and construction

techniques should be incorporated to help protect residences within these areas.

- iii.) All structures built within high wild fire hazard areas shall be constructed to prevent fire and meet standards set forth in Fire Safe Guides for Residential Development in California (California Department of Forestry, May 1990, pp. 18-26) and comply with City codes requiring fire resistant roofing materials.
- iv.) On scenic ridgeland areas where a visibility analysis is required by the General Plan Policy Document (as illustrated in Figure 3F), all roofing materials and exterior building surfaces shall be required to appear natural in character (i.e. form, pattern, texture, and color), minimize the use of reflective surfaces, and use only earth tone colors (e.g. tan, terra cotta, brown, green, and gray) with emphasis on darker shades.
- i. Grading and Landscaping
  - i.) Graded slopes shall be promptly replanted with native vegetation to minimize erosion potential and mitigate aesthetic impact. Revegetation with native trees and fire-resistant ground covers and shrubs is required.
  - ii.) On scenic ridgeland areas where a visibility analysis is required by the General Plan Policy Document (as illustrated in Figure 3F), grading and disturbance of existing vegetation shall be limited to areas required for construction and fire safety only.
  - iii.) On scenic ridgeland areas where a visibility analysis is required by the General Plan Policy Document (as illustrated in Figure 3F), all roads and entry drives shall be located and constructed in a manner that minimizes cut and fill.
- j. Fences and Walls
 

Long stretches of tall, opaque fencing detract from Area A's natural beauty and block views, especially along lot lines. Walls and fences also diminish the quality of wildlife habitats by interfering with the movement of wildlife. Limited outdoor enclosures can be desirable, however, in providing for security and privacy, and to separate domestic pets from wildlife and their habitat.

  - i.) Lot line fences and/or fencing of an entire property at the property line is discouraged unless transparent fencing (i.e. wire or chain link) is used.
  - ii.) Fencing materials shall blend with the natural surroundings. Privacy fences shall not exceed 6 feet in height.
  - iii.) Retaining walls shall have a texture that creates shade and shadow on the wall. Flat, non-articulated concrete retaining walls shall be prohibited. In general, retaining walls shall not be greater than 9 feet in height except where required for street rights-of-way.

**Summary of Required Dimensions - The Ridgeland**

Minimum Lot Size	
Single family detached (non-clustered)	40,000 sq. ft.
Development Cluster Overlay Zone	6,000 sq. ft.
Minimum Yard Setbacks	
Front Yard	30 feet
Front Yard	20 feet
with constraining environmental features or clustering	
Side Yard (single family detached)	30 feet
Side Yard (clustered single family detached)	10 feet
Rear Yard	30 feet
with constraining environmental features or clustering	20 feet
Minimum distance between residential buildings	

(attached units, with clustering)	20 feet
Minimum Building Setbacks	
From centerline of Healdsburg Avenue without use of berms, non-habitable structures as buffers, or sound protected habitable structures that meet General Plan standards	170 feet

### **11.4.3 The Gateway**

#### **Policy**

- Establish a sub-area that provides a distinctive primary entrance gateway to Area A at Healdsburg Avenue.

The Gateway is the subarea at the main entrance to Area A, where Area A's primary entry road meets Healdsburg Avenue.

The Gateway is intended to establish a distinctive entrance to Area A as well as complement the scenic qualities of Healdsburg Avenue.

This subarea is divided by the proposed entry road into two areas in which the land use pattern and zoning will be different. The area north of the entry road, adjacent to Healdsburg Avenue and referred to as Gateway North, will feature a cluster of detached single family homes on relatively small lots similar to the Neighborhood Center subarea, while the area south of the entry road, Gateway South, will feature an enclave of high density housing with a distinctive architectural profile.

#### **Objective**

- Use a special landscape treatment of the intersection of the entry road at Healdsburg Avenue to enhance the primary entry into Area A and to screen both roadways and development.

The rural, scenic qualities of Healdsburg Avenue and the entry road will be maintained with open space and landscaping.

#### **Standards**

##### a. Development Pattern

On the north side of the entry road, single family lots of variable size on relatively small lots will form a small neighborhood enclave. On the south side of the entry road, development may be of any type, including detached or attached single family dwellings, condominiums or apartments.

##### b. Dwelling Unit Types

All dwelling unit types and/or mixes of dwelling unit types and configurations are permitted.

##### c. Yards

The scenic character of the Gateway will be established, in part, through the creation of setbacks and landscape screening which also will help protect residents from traffic noise from Healdsburg Avenue and the entry road.

- i.) All habitable structures shall be set back at least 170 feet from the centerline of Healdsburg Avenue to avoid unacceptable noise levels unless project proponents can document that any proposed habitable structures within that setback will meet the General Plan noise standard for residential land use using measures such as placement of berms or non-habitable structures between habitable structures and Healdsburg Avenue, and use of acoustical building design.

- ii.) All yard requirements shall be the same as the Neighborhood Center except as noted in (i.) above along Healdsburg Avenue.
- iii.) Parking and garages should not be visible from the entry road or Healdsburg Avenue.
- d. Fencing
 

Fencing of the perimeter of the properties will be important in creating the gateway image and identity for this subarea.

Open wood fences (especially split rail) or low stone walls are encouraged along property lines to emulate the agricultural fencing found in the countryside around Healdsburg.  
(See Chapter 9 - Circulation Element Figures 9H and 9I)
- e. Landscaping
 

Landscape features will further contribute to the rural sense-of-place of the Gateway subarea and be important in creating the proper image and identity for the entrance to Area A from Healdsburg Avenue.

  - i.) A landscape design that uses orchard or vineyard patterns is encouraged. This pattern of landscaping should be implemented on both sides of the intersection at Healdsburg Avenue and the Entry Road. Blossoming trees or vineyards should be planted in a grid, as if being cultivated, and may be under-planted with mustard or other flowering ground cover. Hedges are also permitted.
  - ii.) Stone walls, fences, trellises, hedges, or other features shall be used or combined between structures to fully screen parking from roads and streets.
  - iii.) A landscaped median shall be provided to further emphasize the entrance and minimize the visual impact of the wide intersection required on Healdsburg Avenue.
- f. Garages and Parking
 

Where attached single or multi-family dwellings are proposed, garages shall be designed as an integral part of the complex of buildings, either as separate ancillary structures or contained within the ground level of buildings. Surface parking shall be broken up by trees and landscaping to reduce the apparent size of the lots and to provide shade.
- g. Building Height and Massing
 

A variety in the height and massing of buildings is desirable.

  - i.) Structures may be up to 2 stories.
  - ii.) A mix of unit types within the Gateway Area is encouraged.
  - iii.) Where multi-family dwellings are proposed, buildings shall have a simple form that consists of a dominant shed-like volume with lesser add-on shed-like volumes.
- h. Exterior Finishes, Entries, Fenestration and Details
 

Consistent use of exterior fenestration, wall materials and colors for all buildings will contribute to a coherent image for the Gateway. Deeply inset (rather than flush) window frames and entries create dramatic shadow lines against the form of the buildings. Also, certain architectural features reoccur in the rural landscape and in historic areas of Healdsburg, such as cupolas, brackets, verandas and porches.

  - i.) The addition of special architectural features, such as cupolas, roof brackets, and simply detailed verandas and porches is encouraged.
  - ii.) The use of deeply inset (rather than flush) window frames and entries is encouraged.
  - iii.) Light, neutral exterior colors are encouraged for exterior finishes.

### **Summary of Required Dimensions - The Gateway North**

Minimum Building Setbacks From Healdsburg Avenue

(See Ridgелands)

Minimum Lot Width

Maximum Lot Coverage

(See Neighborhood Center)

Maximum Building Heights

(See Neighborhood Center)

Minimum Front Yard

(See Neighborhood Center)

Minimum Private Yard Area

(See Neighborhood Center)

Minimum Setbacks for Garages from Street

(See Neighborhood Center)

Minimum Lot Size

(See Neighborhood Center)

Maximum allowable dwelling unit square footage

(See Neighborhood Center)

### **Summary of Required Dimensions - The Gateway South**

Minimum Building Setbacks From Healdsburg Avenue

(See Ridgелands)

Minimum Front Yard

(see Neighborhood Center)

Minimum Side Yard

(See Neighborhood Center)

Minimum Rear Yard

(See Neighborhood Center)

Maximum Height

2 stories

Maximum Length

90 feet

## **11.4.4 NEIGHBORHOOD EXTENSION**

### **Policy**

- Extend the development pattern of existing neighborhoods that border Area A to the south.  
The Neighborhood Extension will continue the development pattern characteristic of existing development to the south.

This subarea is divided by a proposed extension of Rosewood Drive into two areas featuring differences in land use patterns based on response to topography. The relatively level area to the west of this proposed extension adjacent to Healdsburg Avenue, Neighborhood Extension West, will have greater lot size diversity and smaller lot patterns, similar to the Neighborhood Center. The area east of the proposed extension, Neighborhood Extension East, will have somewhat larger lots on sloping land which forms a transition between the existing neighborhood to the south and the ridgелands.

Single-family homes will predominate. Homes will clearly address the street with front yards, porches and entries. The visual and physical impact of garages will be minimized by encouraging detached garages located in the rear of lots or, if attached, the impacts will be mitigated through design treatment.

### **Objective**

- Use single-family detached houses with lots of similar size to blend new development with existing neighborhoods.

The Neighborhood Extension's preferred development pattern consists of single-family detached houses. Attached garages near the front of residences will be permitted, however, because of the proximity of similar residences just to the south. The negative impact of such garages should be mitigated, however. Detached garages are encouraged.

### **Standards**

- a. Development Pattern  
Houses shall be oriented toward the street. The placement of garages at the rear of lots is encouraged. The visual impacts of attached garages near the front of residences shall be mitigated through project design.
- b. Development Standards and Guidelines  
All guidelines pertaining to the Neighborhood Center shall be applied to the Neighborhood Extension, except:
  - attached garages shall be permitted with mitigations as described in the Neighborhood Center guidelines for attached garages
  - Housing types described in the Neighborhood Center guidelines are permitted.
- c. Setback from Healdsburg Avenue  
All lots adjacent to Healdsburg Avenue shall maintain a minimum building setback as required in the Ridgeline and Gateway subareas.
- d. A buffer or other appropriate means will be required along the southern boundary of the Neighborhood Extension subarea where it adjoins the north side of Pheasant Drive in order to avoid a severe elevational change and the need to provide a high retaining wall with unacceptable safety and/or aesthetic characteristics. The appropriate means for achieving this objective should be proposed once a tentative map is submitted for the development of this area.

### **Summary of Required Dimensions - Neighborhood Extension West**

Minimum Building Setback From Healdsburg Avenue

(See Ridgelines)

Minimum Lot Width

(See Neighborhood Center)

Maximum Lot Coverage

(See Neighborhood Center)

Maximum Building Heights

(See Neighborhood Center)

Minimum Front Yard

(See Neighborhood Center)

Minimum Private Yard Area

(See Neighborhood Center)

Minimum Setbacks for Garages from Street

(See Neighborhood Center)

Minimum Lot Size

(See Neighborhood Center)

Maximum allowable dwelling unit square footage

(See Neighborhood Center)

### **Summary of Required Dimensions - Neighborhood Extension East**

Minimum Lot Width

at front yard and rear yard

50 feet

Maximum Building Height

35 feet

Minimum Front Yard

20 feet

Minimum Side Yard

5 feet - 1 story, 10 feet - 2 story

Minimum Rear Yard

25 feet

Minimum Lot Depth

90 feet

Minimum Lot Size

6000 square feet

### **11.4.5 PARKS AND STORMWATER DETENTION**

#### **Policy**

- Integrate natural features into the design of parks and detention facilities that make up a large part of the Neighborhood Center. Natural features should be integrated with their overall design.

#### **Objective**

- Ensure that man-made features are consistent with Area A's small town and rural character.

#### **Standard**

- a. All grading standards shall be met in Section 11.3.
- b. Lighting for playing fields and courts shall be designed to avoid glare and light spillover to nearby residential areas and the open space setback along Healdsburg Avenue. Height and cutoff angle of luminaires, and the height and density of vegetation, shall ensure that private yards and an area within 125 feet of Healdsburg Avenue not be lit directly by park lights.
- c. Lighting along paths shall be designed to minimize glare and light spillover. The height of luminaries shall not exceed 12 feet and their cutoff angle shall be adjusted to light only the path.
- d. Encourage the use of wood or wood and wire fencing and landscaping to restrict or direct movement, rather than chainlink fencing. The height and character of fences shall be consistent with Standard d, Section 11.4.3, The Gateway.

### **11.4.6. HISTORICAL FEATURES**

#### **Policy**

- Protect historic features that are an important resource to the area's past and contribute to the area's sense-of-place.

#### **Objective**

- Restore or adaptively re-use historic buildings and landscape features where feasible. New buildings surrounding structures of historic significance shall be aesthetically compatible in size, massing, color, detail and materials.

## **12. IMPLEMENTATION ELEMENT**

# 12 IMPLEMENTATION

The preceding chapters of this Specific Plan provide the plans, policies and guidelines for the orderly development of Area A. This chapter sets forth a variety of implementing steps and regulatory and organizational procedures to implement the Specific Plan.

## **12.1 SUMMARY: SPECIFIC PLAN IMPLEMENTATION PROGRAM**

The following shows the approximate sequence of the key implementing steps that should be followed by the City to effectively implement this Specific Plan.

- Certify the Area A Specific Plan Program Environmental Impact Report
- Adopt findings as required by the California Environmental Quality Act (CEQA)
- Amend the General Plan
- Adopt the Specific Plan
- Adopt rezoning for the Specific Plan area
- Conclude property tax exchange agreement with the County
- Prepare a Plan for Services
- Annex Specific Plan Area into the City
- Adopt development review procedures for projects in the Specific Plan area
- Review and approve individual Tentative Subdivision maps
- Adopt Public Improvement Plans
- Adopt Financing Plans
- Negotiate development agreements and set up financing mechanisms (assessment districts, impact fee ordinance, etc.)

## **12.2 KEY IMPLEMENTING ACTIONS OF THE SPECIFIC PLAN**

### **12.2.1 EIR CERTIFICATION**

To meet the requirements of the California Environmental Quality Act (CEQA), a program Environmental Impact Report (EIR), as authorized by Section 15168 of the CEQA guidelines, has been prepared to assess the environmental impacts of the Area A Specific Plan.

The Specific Plan's EIR has been prepared as a program EIR to cover the development of Area A as a total undertaking, although development is expected to occur in several increments over a number of years by several developers. The program EIR will expedite the processing of future projects. Projects that are consistent with the Plan, and are determined by the City's Initial Study to not result in new effects or require additional mitigation, may be approved without further environmental documentation. For other projects, only those with potential impacts will require additional analysis. The Specific Plan's Final EIR must be certified by the City as accurate and complete, once all comments on the Draft EIR have been responded to. The City's action to certify the Final EIR does not constitute approval of the Specific Plan. Rather, it indicates that all required environmental information has been presented to the City's decision-makers and the public.

### **12.2.2 CEQA FINDINGS**

The City must approve Findings of Fact and Statement of Overriding Considerations, as required by Sections 15091 and 15093 of the CEQA guidelines. The Findings of Fact explain how the City has dealt with each significant adverse environmental impact and the project alternatives offered in the Final EIR. The Statement of Overriding Considerations identifies the specific reasons for approving a project for which all significant adverse environmental impacts have not been at least substantially mitigated. These findings must be approved prior to the adoption of any General Plan amendments and the Specific Plan.

### **12.2.3 GENERAL PLAN AMENDMENTS**

The City will amend the Healdsburg General Plan to add appropriate land use designations for Area A. Amendments to the General Plan's policies and text will also be adopted.

The Healdsburg General Plan currently designates the Northern Planning Area for a combination of medium density residential (3-6 dwelling units/acre), very low density residential (0-1 dwelling units/acre), and open space. With adoption of the plan, the City should revise its General Plan Land Use Map to show the distribution of land use categories represented in the Specific Plan Land Use Map (Figure 6A). The Specific Plan introduces one land use category which is not currently used in the General Plan: Medium-High Density Residential (6-8 du/ac).

The City will also amend the Healdsburg General Plan to incorporate the circulation plan shown in Figure 9B and the street standards shown in Figures 9C, 9D, 9E, 9F, 9G, 9H, 9I, and 9J, as well as associated policies and text.

Finally, Figure 11-3 in the General Plan Policy Document will be amended to show the proposed park site on the boundary of Areas A and C adjacent to the detention basin, and to delete a potential view park site in the north central portion of Area A.

### **12.2.4 ADOPTION OF THE SPECIFIC PLAN**

Following amendment of the General Plan, the Specific Plan will be adopted by the City Council.

### **12.2.5 PREZONING**

The City has determined the appropriate zoning applicable to Area A in anticipation of subsequent annexation of this area to the City. Such prezoning becomes the zoning of property when the annexation becomes effective. Prezoning of Area A in relationship to residential densities is shown in Figure 12A.

In some cases, the existing zoning ordinance will be sufficient for application to proposed land uses and densities in Area A. However, the Specific Plan will require amendment of the zoning ordinance to provide a new residential district with a minimum lot size of 3,500 square feet. In addition, the Specific Plan will require amendment of the zoning ordinance to provide a new overlay zone which implements the General Plan policy of encouraging clustering and density transfer: the Development Cluster Overlay zone. In Area A, this overlay zone is applicable only to lands designated Very Low Density Residential. It permits variable lot size, with a minimum lot size of 6,000 sq. ft., as long as the total area does not exceed a maximum density of 1.3 dwelling units per acre, as well as any housing types. This overlay zone may be requested as part of an application for development on lands where applicable for any one or more adjacent properties where development is proposed. Once this zone is established, no properties within this zone can be further subdivided if it would result in exceeding the maximum density permitted for the overlay zoning district as a whole.

The boundaries of another overlay zone allowing up to 20 units per acre (with State mandated density bonus) for certain qualified low income housing projects, referred to as the AH-1 overlay zone, are designated in the Specific Plan land use map. In addition to sites in Area A, a number of sites within the existing City limits of Healdsburg will be designated AH-1 overlay zones as part of the implementation of the City's Housing Element goals and policies.

City rezoning of the Planning Area should be consistent with the amended General Plan. The following table (Table 12.1) should be used as a basis for rezoning the area.

A portion of Area A is zoned P - Public. This area will contain the detention basin and lands intended for the construction of a park. The detention basin has been designed and will be constructed in the first phase of construction in Area A.

When the final design for the park has been determined, this area should be zoned RP -Recreation and Parks. If it is determined that a school site is not required in the future, the land held for the school may be rezoned for HR - High Density Residential.

Figure 12A

**Table 12.1  
RECOMMENDED REZONING FOR LAND USE DESIGNATIONS**

<b>Specific Plan Land Use</b>	<b>City Zoning District</b>
Very Low Density (0-1 du/ac)	RI - 40,000 sq. ft. minimum
Medium Density (3-6 du/ac)	RI - 6,000 sq. ft./RI - 3500 sq. ft.
Medium High Density (6-8 du/ac)	RI - 3500 sq. ft.
High Density	R-M
Public	P - Public

In addition to the above land use designations, two overlay zones will be implemented in portions of Area A. These include the following:

- Development Cluster Overlay Zone (0-1.3 du/ac) CD 6000 sq. ft. minimum
- Affordable Housing Overlay Zone (12-20 du/ac) AH-I

In Area A, the development cluster overlay zone will be applicable only to the RI -40,000 zoning district, whereas the two AH-I overlay zones are proposed in areas on sites in which the "base" zone will be R-M (Multi-family Residential).

### **12.2.6 ANNEXATION**

The Area A Specific Plan area is within the City's current sphere of influence, outside the existing city limits. The City will be required to adopt a resolution of annexation before property owners file an application for annexation with the Sonoma County Local Agency Formation Commission.

Although it would seem logical that all of Area A should be annexed at one time, this strategy may be impractical given the numerous property owners and the number of developed parcels which may not benefit from annexation. It is anticipated that it may take 10 years or more for the area to build out and may not be financially feasible for a person owning property in one of the later phases to pay the annexation fees years in advance of submitting a development proposal.

Therefore, the City intends to annex property as property owners request annexation, so long as the annexations are consistent with the City's General Plan policy which states, "The City shall discourage annexations that would result in the creation of unincorporated islands, peninsulas, or other irregular boundaries, provided that such restrictions would not be detrimental to planned growth and development."



### **12.2.7 SUBAREA DEVELOPMENT LIMITS**

The Specific Plan is based on providing the necessary infrastructure and improvements for a maximum of 600 dwelling units. The Specific Plan furthermore divides Area A into sub -areas. In order to ensure that development does not exceed infrastructure or environmental constraints, limitations on the number of dwelling units, by either subarea or portion thereof, are established. Maximum units per subarea or portion thereof shall be as shown in Figure 12B. Subareas are further divided in Figure 12B to separate those portions of subareas based on existing separate ownerships (for example, in the Ridgeland where a property currently owned by Hearn is surrounded by property currently owned by ZAM Enterprises, and in the Gateway where the north portion is currently owned by ZAM and south portion is currently owned by Banducci), or where boundaries between subareas extend across existing property boundaries (for example, property currently owned by Vinecrest Partnership). Subareas are also divided where different zoning districts are proposed (for example, on property owned by Vinecrest Partnership and Gomez where both R-1-6000 and R-1-3500 is proposed for the same subarea).

The boundaries between subareas and the portions thereof, are considered approximate. In some cases, final determination of boundaries as shown in Figure 12B will be based on more precise mapping of fault traces and plans for the detention basin and public park. Therefore, where more than one subarea is shown within and existing single ownership, the numbers limiting how many dwelling units may be built in each subareas can shift, as long as the total number of units allowed for each single ownership with more than one subarea is not exceeded.

These maximum dwelling unit allocations are consistent with amended General Plan land use designations. Maximum dwelling unit numbers shown in Figure 12B

### **12.2.8 PHASING OF DEVELOPMENT**

Five phases of development are anticipated for Area A:

Phase 1 - All improvements required to widen and improve Healdsburg Avenue.

Phase 2 - Development of the Gateway Area, areas north of the entry road to the Area A property line except for a small portion of land in the northeast corner of the site. The Neighborhood Extension and lands immediately east of this area are also a part of this phase.

This phase includes the construction of the detention basin.

Phase 3 - Development of the remaining lanes north of the entry road and most properties immediately south and adjoining the road.

Phase 4 - Development of most lands immediately north of Phase 2 at the Area A southern property line, accessed via Rosewood Street and Poppy Hill Drive, lands immediately south and adjoining Phase 3 lands and a small area accessed via El Arroyo Drive.

Phase 5 - Development of the remaining lands in Area A, primarily those areas fronting on Healdsburg Avenue between Phases 2 and 3 and hilly areas surrounded by Phase 4.

### **12.2.9 PUBLIC IMPROVEMENT PLANS**

The on-site and off-site public improvements necessary to serve Area A must be specifically designed. The applicants should prepare for City review and approval Public Improvement Plans, consisting of detailed engineering designs and documents for all utilities necessary to develop the land uses identified in the Specific Plan. These plans will be consistent with the Phasing of Capital Improvements plan found in Section 12.2.10 so that the phasing program will be used to allow for orderly development throughout the Specific Plan area.

## 12.2.10 PHASING OF CAPITAL IMPROVEMENTS

It is assumed that development in Area A will occur over a number of years. Some capital improvements must be made initially to the Area before or concurrently with the first phase of development. Other improvements can be constructed as subsequent phases of construction take place. Detailed descriptions of all the public facility systems required both on- and off-site are contained in Chapter 8 - Circulation Element and Chapter 9 - Public Facilities Element.

### a. Phase I Improvements

The primary capital improvements required for the Phase I development are drainage, sewer, water, public services and roadway improvements, especially to Healdsburg Avenue. These improvements are located both on and off-site of Area A.

#### i.) Drainage

The primary drainage improvement associated with development of Phase I is the North Detention Basin which will be located both on and off-site of Area A. The basin will be constructed just upstream of the crossing of Foss Creek under Healdsburg Avenue at the northwest corner of Area A. This detention basin must be constructed to mitigate increased runoff to Foss Creek resulting from development in Area A, as well as other downstream drainage impacts. The entire basin must be constructed in Phase I.

Phase I construction of the Neighborhood Extension and the adjoining portion of the Ridgeland will require implementation of applicable storm drain outlets from Healdsburg Avenue to Foss Creek. Drainage from development in the areas of the Ridgeland in the northwest corner (west of the ridgeline) will be included in roadway improvements in Healdsburg Avenue.

#### ii.) Sewer

Because Phase I development will take place on both sides of the ridgeline, sewer improvements outlined in the *Pre-Design Report for the City of Healdsburg North Area Off-Site Sewer Improvements* by Brelje & Race will be required (see Chapter 10). Initial sewer improvements will provide sufficient gravity capacity for development west of the ridge and will extend sewers in Healdsburg Avenue to the vicinity of the North Detention Basin so they may be extended to the Neighborhood Center area and provide gravity service for development east of the ridgeline. From this point, the on-site sewer system will follow the drainage improvements in the Area A roadway system.

#### iii.) Water

Existing lines in Healdsburg Avenue will be extended to Area A roughly paralleling drainage and sewer improvements or in new street rights-of-way constructed to serve development. Off-site, an 800,000 gallon storage reservoir near the Gauntlett Reservoir site and the development of the Dry Creek Well Field (or alternatively, additional wells at the Fitch Mountain Well Field) will be required in Phase I.

#### iv.) Public Services

Except for electric services, no other public services will require special improvement beyond extension of these services to the site during construction.

The existing 60 kv line which traverses the site will be relocated on poles to follow a new street right-of-way in the Neighborhood Center.

#### v.) Roads

Significant off-site roadway improvements will be required to serve Phase I development. These are:

- widening of Healdsburg Avenue between Dry Creek Road and the main entry road to Area A to 5 lanes;
- widening of Healdsburg Avenue north of the Area A entry road to 3 lanes;
- modifications to traffic signals or new traffic signals required by the above improvements.

Construction of all these improvement should be coordinated with the installation of all Phase I underground

infrastructure mentioned previously for Area A and beyond to serve Area C. The special constraints imposed by the proximity of Foss Creek and the adjacent hillside in the northwest corner of Area A add further to the requirement for close coordination and perhaps simultaneous construction of roadway and utility substructure improvements.

On-site, the entry road will be constructed extending to the Neighborhood Center. All other Neighborhood Center roads north of the entry road will also be constructed to provide a "loop" roadway system to serve Area A for fire and police access.

b. Phase 2 and Phase 3 Improvements

Since most major on- and off-site improvements will be made in Phase 1, Phase 2 and Phase 3 infrastructure services will be extended from these improvements within street rights-of-way to serve development.

In Phase 2, the entry road will be extended to the Area A border with Area B.

The two roads that traverse the Ridgeland in a north/south direction will be constructed in Phase 3. Booster pumps will be required for any dwellings located above 320 feet in elevation in the Ridgeland.

### **12.2.11 TENTATIVE MAP**

The subdivision process in the Planning Area will be governed by the Subdivision Map Act, as well as City standards and procedures. Tentative maps must be consistent with the Specific Plan.

### **12.2.12 FINAL MAP**

When all issues associated with the Tentative Map are resolved, a Final Map will be filed and approved by the City, in keeping with City standards and procedures, and the Subdivision Map Act.

### **12.2.13 PARK IMPROVEMENT PLANS**

The City shall prepare improvement plans for proposed public parks.

### **12.2.14 FINANCING PLANS**

The major capital improvements required to support development in Area A area, major project responsibilities and possible methods of funding are described in Chapter 13. Detailed financial plans shall be prepared and be made a part of any Development Agreement, if such an agreement(s) is desired. The Financing Plans shall identify the necessary capital improvements including public facilities, streets and utilities and assure their timely financing. Implementation of the Financing Plans can be assured by inclusion of provisions in development approvals and/or development agreements that require adherence to the plan.

### **12.2.15 RESPONSIBILITIES FOR KEY IMPLEMENTING ACTIONS**

The following table indicates the responsibilities for preparation of the documents discussed above:

**Table 12-2**  
**RESPONSIBILITIES FOR KEY IMPLEMENTING ACTIONS**

<b>Key Implementing Actions</b>	<b>Preparation</b>	<b>Adoption by</b>
• EIR Certification	City	City
• CEQA Findings	City	City
• General Plan Amendments	City	City
• Specific Plan Adoption	City	City
• Rezoning	City	City
• Annexation	Developers	LAFCO, City
• Tentative Map	Developers	City
• Site Development/Design Review	City	City
• Public Improvement Plans	Developers	City
• Final Subdivision Map	Developers	City
• Park Improvement Plans	City	City
• Financing Plans	City	City

### **12.3 OTHER IMPLEMENTING ACTIONS**

In addition to the foregoing key actions, the following actions will assist in Specific Plan implementation.

#### **12.3.1 DEVELOPMENT AGREEMENTS**

The City may require applicants for development in Area A to enter into a mutually-acceptable development agreement with the City for their respective area. Agreements should only be arranged where the developer is prepared to proceed promptly in accordance with a specific time schedule for seeking the required approvals and commencing construction. Typically, the agreements would be entered into after the EIR is certified and before tentative subdivision maps are approved.

Such development agreements will set forth the roles that will govern the developments as they proceed through the approval process. Both the City and the project sponsors (developers) would commit themselves to proceed in accordance with the terms of the agreements. The City may agree to process further applications in accordance with its plans and laws in existence at the time of the agreements. In effect, the City promises not to change its planning or zoning laws applicable to these developments for a specified period of time. Thus, future land use decisions are not made according to the City's laws and policies in effect at that time, but are made according to the laws in effect when the agreements were entered into. In return, the developers may agree to construct specific improvements, provide public facilities and services, develop according to a specified time schedule or make other commitments which the City might otherwise have no authority to compel the developers to perform.

The Specific Plan and its EIR places substantial requirements on the development of the properties within Area A. These requirements include financing, construction and maintenance of public facilities, design standards, and mitigation of environmental impacts. For this Specific Plan, a development agreement is the recommended legal document to:

- augment the City's standard development regulations in response to the particular characteristics of each project;
- Spell out the precise financial responsibilities of the developer;
- Ensure timely provision of adequate public facilities for each project;
- Streamline the development approval process by coordinating various discretionary approvals;
- Provide the terms for reimbursement when a developer advances funding for specific facilities which have

community-wide or area benefit;

- Provide mutual certainty to both the City and the developer regarding entitlements to the developer in return for commitments for public improvements.

The City should first develop a Master Development Agreement to serve as the format for all development agreements within Area A. The conditions included in this Master Development Agreement would then be tailored to the special condition for each major project area and the development projects within it.

## **12.4 ADMINISTRATION OF THE SPECIFIC PLAN**

When the Area A Specific Plan is adopted by the City Council, it will be used to direct the processing of proposed projects within the Planning Area. Given that many developers may be involved in the development of Area A, the following responsibilities, mechanisms and procedures will be necessary to review, monitor, coordinate and integrate what otherwise may be piecemeal and uncoordinated incremental development.

### **12.4.1 RESPONSIBILITIES FOR ADMINISTRATION OF THE SPECIFIC PLAN**

Administration of the Area A Specific Plan will be a joint effort of the City of Healdsburg and any developer proposing to develop in Area A or is part of a development agreement that has been negotiated with the City.

### **12.4.2 TYPICAL DEVELOPMENT REVIEW PROCESS**

The intent of this section is to summarize the typical procedural steps needed to review and approve projects in Area A. The following discussion of the development review process is simplified. Detailed information on how a proposed project can be processed should be obtained from the Healdsburg Planning Department.

#### **Summary of the Development Review Process**

- A proposed project (usually a subdivision map or a development plan) is submitted to the Healdsburg Planning Department for processing.
- Where the property includes prescribed environmentally sensitive areas (i.e., riparian woodlands, natural wetlands, serpentine chaparral, historic structures, archaeological sites, and landslide or fault zones) prohibited from development or visually prominent ridgelines identified in Appendix A of the Plan, the applicant will provide mapping at a larger scale than provided by the Specific Plan to accurately locate such areas and viewsheds in relation to proposed building sites, roadways, fuelbreaks, and improvements. Precise locations of specific resources protected by the Plan could not be finally established at the scale of the mapping used for the Specific Plan. This precision must be provided when the application for subdivision and development

#### **Responsibilities**

Land Owners/Developers

Land Owners/Developers

is made. After submittal of such mapping and any environmental analysis by qualified experts as provided by the applicant, the location of mapped boundaries of the prescribed environmentally sensitive areas can be adjusted to conform with the findings of the analysis.

Applicants may also propose measures to mitigate development impacts to a less-than-significant level. Where impacts would be inadequately mitigated, the applicant will be required to have a focused EIR prepared.

- Where the property includes area where detailed geotechnical investigations are required by the Specific Plan (see Section 6.5 Conservation and Public Safety, Standard a.) and the General Plan, the applicant will be required to submit pertinent information. Land Owners/Developers
- Where the property is within 170 feet of Healdsburg Avenue and development is proposed within the 170' noise setback required by this Plan, the applicant will provide documentation that proposed design measures for the project will reduce noise levels to General Plan standards for residential development. Land Owners/Developers
- Where the property includes oak woodlands or heritage trees, the applicant will be required to submit plans to demonstrate how such trees will be protected, or loss will be mitigated, based on the standards of this Plan. (See Section 8.2 Environmental Protection and Resource Conservation, Standards g., h., and i.) Land Owners/Developers
- If the proposed project is located in the Ridgeland and involves a request for clustered development of variable lot size under the provisions of the Cluster Development Overlay Zone, the applicant will be required to submit information as needed to demonstrate compliance with the provisions of this zoning district. Land Owners/Developers
- If the proposed project is next to and involves alterations to a natural waterway or wetland, the applicant may be required to submit pertinent information to agencies having jurisdiction over these resources. Calif. Dept. of Fish & Game  
US Army Corps of Engineers

- If a proposed project involves the dedication of parkland or development of a park, creek or other open space area, pathway or trail, it must be reviewed for consistency with this Specific Plan as well as the needs of the wider community. Parks and Recreation Commission  
Planning Department
- If a proposed project involves area subject to wildland fire risk, the location of fuel breaks shall be shown on the tentative map or development plans. Land Owners/Developers
- Each proposed project shall be reviewed by staff for conformance with City land use laws, engineering standards and the provisions of the General Plan and this Specific Plan. Planning Department  
Public Works Department
- An initial environmental study will be prepared for each proposed project. Such environmental review will determine if the proposed project is consistent with the provisions in the Specific Plan to protect the environment, including siting criteria for proposed development, whether all important environmental issues were adequately addressed by the EIR for this Specific Plan or whether additional study is needed. Projects which do not conform with the standards of this Specific Plan nor propose mitigation adequate to address impacts may require a focused EIR prior to project approval. Planning Department
- Site development/design review for projects within the Planning Area as required in this Specific Plan. Planning Department  
Planning Commission
- Each project will be reviewed for its consistency with the provisions of the General Plan and this Specific Plan. Based on findings, it will be approved, changed or denied. Planning Department  
Planning Commission  
City Council

### **12.4.3 SPECIFIC PLAN CONSISTENCY**

Following adoption of this Specific Plan, no development plan, subdivision, use permit or other entitlement for use shall be approved by the City and no public improvement shall be authorized by the City for construction in Area A until a finding has been made that the proposed entitlement or public improvement is in substantial conformance with this Specific Plan. Approval of final development plans, on-site public improvement plans and use permits also shall be substantially consistent with the applicable provisions of the Healdsburg General Plan.

All Specific Plan changes (both minor and major amendments) must be found consistent with the Healdsburg General Plan, or a General Plan Amendment may be required.

If any regulation, condition or portion of this Specific Plan is held invalid by a California or Federal court, these portions shall be deemed separate, distinct, and independent provisions. The invalidity of these provisions shall not affect the validity of the remaining parts of the Specific Plan.

#### **12.4.4 SPECIFIC PLAN AMENDMENT**

Amendments to the Specific Plan may be requested by a developer or property owner and may be initiated by the City in accordance with City procedures for initial Specific Plan adoption. Specific Plan amendments shall be processed in accordance with City ordinances.

#### **12.4.5 ENVIRONMENTAL REVIEW**

The EIR prepared for this Specific Plan assesses the expected effects of the ultimate environmental changes resulting from the Specific Plan and development taking place in conformance with the Specific Plan. The program EIR will act as a "master EIR" for the specific plan area, reducing the need for project-specific environmental review in the initial years of development under the recommendations of the Specific Plan. Pursuant to Section 15182 of the CEQA Statutes and Guidelines, residential projects which are in conformity with the Specific Plan are exempt from subsequent environmental review, eliminating the need for additional EIR's. For all proposed development projects that conform to the level of development prescribed in the Specific Plan, the subsequent environmental review process will only need to address the project's site-specific impacts. If additional impacts are identified and a subsequent EIR is required, general impacts which are addressed in the Specific Plan program EIR can be included by reference. Overall, the program EIR should result in faster processing of project applications that are consistent with the Specific Plan and the mitigation measures identified in the EIR.

#### **12.4.6 CONDITIONS, COVENANTS, AND RESTRICTIONS**

The major project developers or successors in interest shall be obligated to maintain architectural, landscape and site control at point of sale to insure the cumulative character intended by the Specific Plan and subarea plans. Although Conditions, Covenants, and Restrictions (CC&R's) lie outside City enforcement procedures, this Specific Plan requires the use of CC&R's to enforce the design guidelines of the Specific Plan and to maintain landscape and open space areas and the improvements of each development project. The mechanism(s) to enforce the CC&R's shall be agreed to by the Developers and the City.

All CC&R's shall reflect the requirements contained in the Specific Plan. In addition, provisions for the design and maintenance of fencing, landscaping and open space areas and other facilities within projects as well for the abatement of nuisances shall be set forth in the CC&R's.

Similarly, land owners and/or developers shall be responsible for preparing and recording deed restrictions on any proposed lots in the Ridglands where environmental resources are intended to be protected. Such lots may include areas to be owned and maintained by a homeowner's association or a landscape and lighting district, or individual properties with building or development envelopes. Such deed restrictions shall prohibit development and activities that would degrade the environmental resource to be protected, with the exception of activities required for public safety (i.e., fire hazard reduction), and shall be reviewed by the City before final approval and permits are granted. At the point of sale, the developer is required to disclose to potential buyers any deed restriction required on such land to protect natural resources.

# **13. FINANCING PLAN**

**Prepared By**

**Kurt Hahn  
Director of Finance  
City of Healdsburg**

**and**

**Fred Bengs  
Administrative Analyst Intern  
City of Healdsburg**

# **13 FINANCING PLAN**

The Financing Plan was published as Volume 2 of the Draft Specific Plan in 1993 and will be updated as part of the City of Healdsburg's 25-year Capital Improvements Plan and as part of the developer agreements with the lead developers.